

Design Guidelines for  
Old Town Historic District and Major Thoroughfares

City of Lake Worth, Florida



Historic Postcard

Abell Garcia Architects

# Design Guidelines for Old Town Historic District and Major Thoroughfares

City of Lake Worth, Florida

May 2001

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These Guidelines include the following:

Background information on the development, history, architecture and landscape character of Lake Worth.

Design Guidelines to assist each property owner in preservation of architectural heritage; recommendations for new construction and renovation that respect the community of Lake Worth.

Essential reading for all architects, builders, real estate agents and persons planning to build or purchase property and residents who wish to maintain and renovate their properties.

## **ACKNOWLEDGEMENTS**

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**Many people assisted with the preparation of this report, including the following:**

Lake Worth City Commission

Lake Worth Planning and Zoning Board

Lake Worth Historic Resources Preservation Board

City of Lake Worth Planning Department

Lake Worth Historical Museum

Chamber of Commerce

Downtown Merchants Association

Dixie Highway Merchants Association

Lake Worth Library

State of Florida, Division of Historic Resources, Bureau of Historic Preservation

The RMPK Group

### **Written Resources:**

National Register of Historic Places Nomination for Old Town Historic District

Historic Property Survey of the City of Lake Worth

Model Guidelines for Design Review – Division of Historical Resources, Florida Department of State

## **Design Guidelines**

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## **DESIGN GUIDELINES FOR NEW AND RENOVATED DEVELOPMENT IN OLD TOWN LAKE WORTH HISTORIC DISTRICT AND ALONG MAJOR COMMERCIAL THOROUGHFARES**

### **INTRODUCTION - GOALS**

Abell Garcia was commissioned in the spring of 2000 to develop Guidelines for two major areas of interest to the community of Lake Worth, Florida. In developing these guidelines the consultant reviewed prior planning documents developed for the City of Lake Worth, met with planning staff and held joint meetings with residents, merchants and the City's Community Redevelopment Area Consultant to gain insight and information on the Downtown Historic District and the thoroughfares.

The City of Lake Worth was founded in 1912. Growth was rapid from the platting of the City in 1912 to 1925. As with most development in Florida there was a period of years from late 1925 to the 1940s when little change took place. Over the years from 1940 to the present a series of changes has occurred in the City. During the past ten years, a revitalization of downtown has taken place. In the summer of 2000, the Florida National Register Review Board recommended that most of downtown Lake Worth be declared a National Register Historic District known as the Old Town National Register Historic District (Map 1). There was also a boundary established which denotes the Old Town Local Register Historic District (Map 2). Guidelines for the area are based on the State of Florida Guidelines and the Secretary of the Interior's Standards for Rehabilitation and apply to both new and renovated development. These guidelines comprise the second section of this document.

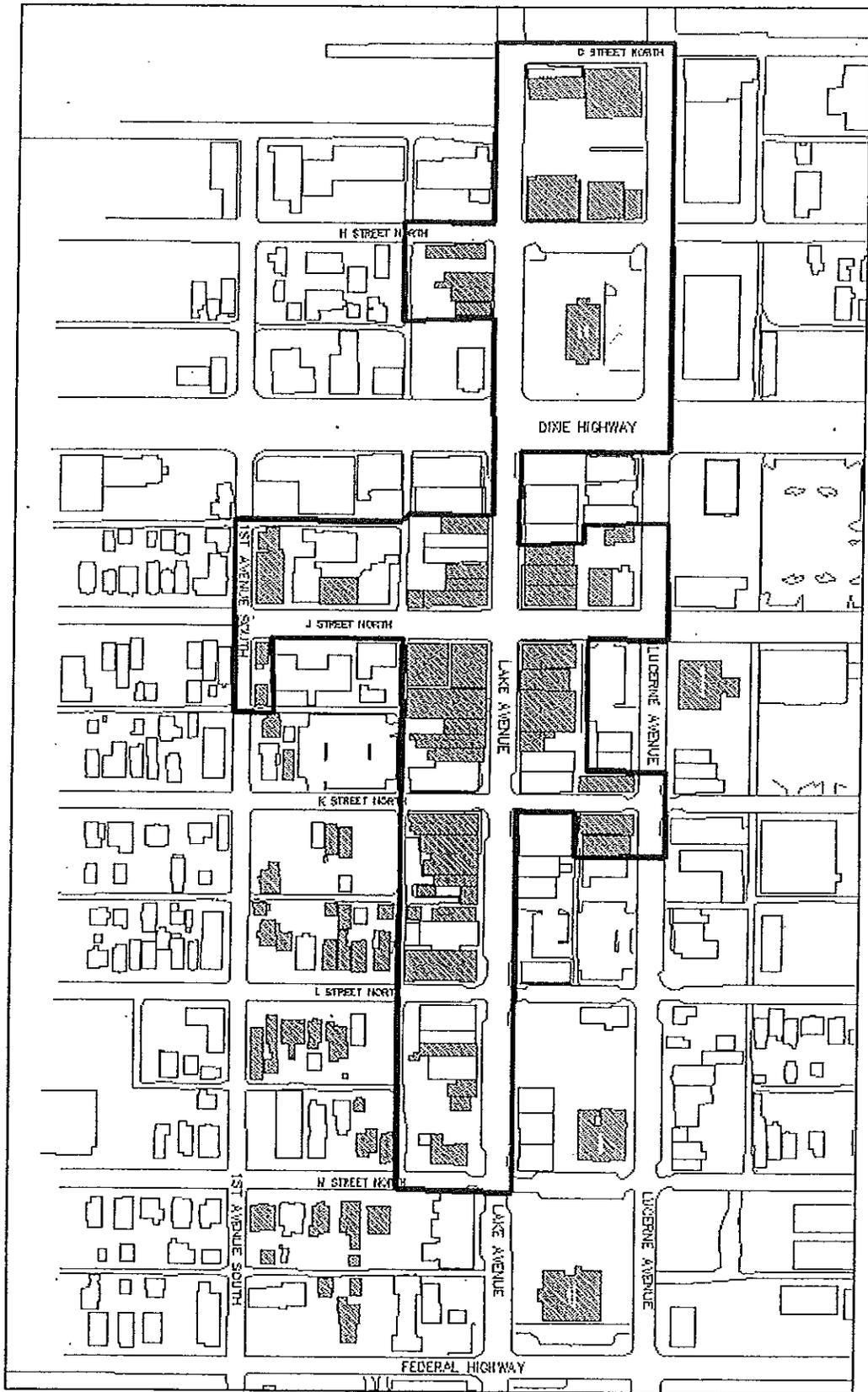
The Design Guidelines for major thoroughfares include the following areas, which are the major routes by which people access Lake Worth and are also the center of major commercial activity:

- Lake and Lucerne Avenues from the Intracoastal to I-95 and within the Old Town Historic District
- H, J, K, L and M Streets within the Old Town Historic District
- 10th Avenue North from Dixie Highway to I-95
- 6th Avenue South from Dixie Highway to I-95
- Federal Highway from the South City Limit to the South Boundary of College Park
- Dixie Highway from the South City Limit to 10th Avenue North

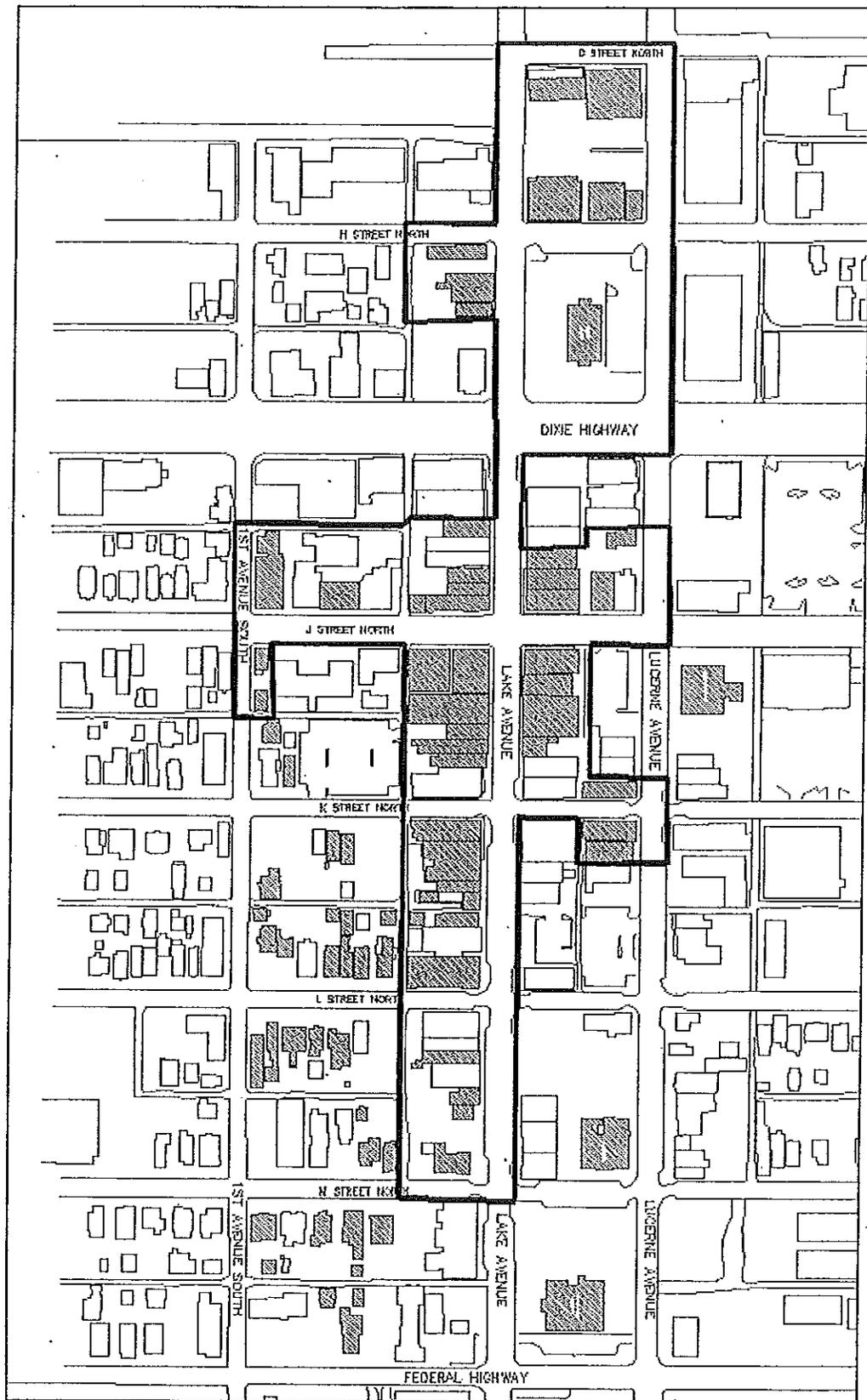
Guidelines for the major thoroughfares are complementary to the guidelines for the Old Town Historic District. These guidelines comprise the third section of this document.

These Design Guidelines will address and integrate the nature of the original development, the Post War development of the 1940s, and the issues resulting from contemporary life styles, including traffic, parking, planting, signage and various other concerns and priorities of the people living and working in Downtown Lake Worth and along the major commercial thoroughfares.

The development of the Design Guidelines offers a guide for the residents and City when considering future development and/or renovations to existing properties in the above areas.



Map 1: Old Town National Register Historic District



Map 1: Old Town National Register Historic District

## PLANNING PROCESS

The consultants working on the Design Guidelines made a series of visits to the Lake Worth area beginning spring 2000 and continuing over a one-year period. In June and August of 2000 this consultant also participated in meetings with the Community Redevelopment Area (CRA) planning consultants. Many of the issues addressed during the planning process for the CRA are equally important to the development of Design Guidelines.

At the June 2000 meeting the Downtown Merchants Association presented a list of concerns to the planning consultants. Design issues on the list included:

- Provision for more parking
- Shade and trees in parking areas
- A desire for more trees and planters along the streets
- Comprehensive signage program
- Improvement of the alleys

Comments indicated that signage should indicate such things as directions to parking, the beaches and golf and should contain a strong and positive visual image identifying Lake Worth.

Assets noted in a series of community meetings include:

- Parks, city and regional
- Ocean and beaches
- Historic Downtown
- Community activism and pride, diversity
- Pedestrian orientation of Downtown
- Strong urban form
- Neighborhood schools
- Climate and tropical vegetation
- Recent road improvements and landscaping
- Historic architecture and neighborhood character
- Unique atmosphere of U.S. 1 Dixie Highway
- Good traffic circulation and volume to enhance business



Historic Postcard  
Palm Beach



Alley in Downtown  
Lake Worth

Concerns voiced by the residents and commissioners included:

- Transition between commercial and residential properties
- Control on amount and type of signage
- Neighborhood viability
- Lack of landscaping
- Infrastructure concerns, local streets, alleys, sidewalks and parking
- Neighborhood recreation and open space
- Small lot sizes which make commercial development more difficult

Additional concerns included:

- Traffic
- Continuity with sidewalks/ADA access
- Lack of Code Enforcement
- Presence of above ground utilities
- Need for Public Information about proposed changes

Many of the commercial properties are typically one lot deep and abut residential land uses. Guidelines for properties on the major thoroughfares need to address the interface between the rear of the commercial properties and the residential properties, as well as how these commercial developments interact with the street and adjoining buildings.

Residents of the City also noted that they are proud of the unique qualities of the City of Lake Worth and want the City to retain its unique identity and



Variety As Seen on Dixie Highway  
West Side of 300 Block North Dixie

look. They would like to create design guidelines, zoning changes and public improvements that enhance the quality of development without creating a City that mirrors many others in South Florida.

The eclectic mix of architectural styles, such as Mediterranean Revival, Moorish Revival, Art Moderne and even Frame Vernacular contribute to the unique sense of place. The residents would like to have appropriate options from which to choose that give them the opportunity to be creative, while still encouraging coherence and quality development.

Solutions proposed at these meetings included:

- Themes for Lake and Lucerne Avenues, 6<sup>th</sup> and 10<sup>th</sup> Avenues
- Extend improvements along Lucerne Avenue to the West of Dixie Highway
- Buffer rail line with vegetation
- Regulate quality in architecture, instead of dictating style
- Do not want all the buildings to look the same, do need to have quality appearance
- Accentuate the historic aspects of Dixie Highway
- Create more parking options for Dixie Highway businesses
- Emphasize gateways to the City on Dixie Highway and 6<sup>th</sup> and 10<sup>th</sup> Avenues, make them more welcoming
- Color palette suggestions and facade improvement programs
- Protect and insure park and recreation spaces for the future and more parks provided in the north and central areas of the City

Conclusions from earlier studies such as the City of Lake Worth Redevelopment and Revitalization Plan, the result of a 1992 Design Charrette, were essential resources. Important issues from these earlier planning studies included:

- Creating an identity for Lake Worth
- Redevelopment of downtown
- Revitalization of neighborhoods
- Public spaces redesigned to full potential
- Allowing more dense development in the downtown district
- Pedestrian emphasis for downtown
- Comprehensive design standards
- Preservation of historical buildings
- Public spaces provided in each neighborhood
- Mix of land uses and building types in neighborhoods
- Provision of affordable housing

The consultants walked and drove the streets to gather data and gain an overall character of the current state of the Old Town Historic District and the commercial thoroughfares. Existing building types were documented. Historical information, current data and base maps provided a base line understanding of the existing character of the Historic Districts and thoroughfares. The concerns voiced by the business community and elected officials guided the consultants in their development of concepts. The Old Town Historic District evidences a variety of styles from different points in the City's history.

Four of the major commercial thoroughfares have recently been redesigned and reconstructed by the Florida Department of Transportation, in cooperation with the City. Funding for this project came from several sources, including monies from the City of Lake Worth. Lake and Lucerne Avenues already have been reduced to two lanes of traffic, with traffic-calming devices, landscaping, underground utilities and eleven-foot-wide sidewalks. Dixie Highway has four lanes of traffic with bulb-outs to reduce traffic and speed, landscaping along both sides of the street and major entrance landscaping as one approaches downtown and City Hall. Federal Highway has a unique character, as it consists of a two-lane roadway that passes through both commercial areas and residential districts. This existing redesign of the four major thoroughfares has already addressed many of the important issues with regard to slowing traffic, making the streets more pedestrian friendly, making the spaces visually pleasing by placing utility lines underground, and by planting trees to make walking more pleasant and give a sense of human scale and identity to these streets.



Streetscape along  
Dixie Highway

Another characteristic of Lake Worth is the compact nature of the City, making it possible to walk or bicycle to many destinations. In creating design guidelines an effort was made to make all of the major thoroughfares more pedestrian friendly.

## **NATIONAL REGISTER AND LOCAL REGISTER LISTING**

The Old Town Local Historic District was adopted by Ordinance 99-17. The Ordinance became effective on June 11, 1999. A smaller, core area of the local district was reviewed and recommended for National Register listing by the Florida National Register Review Board on April 6, 2000. A contributing structure within a National Register Review District is recognition of its importance within that district, based on the National Register criteria. Buildings shown with a hatch pattern in Maps 1 and 2 are contributing structures.

Properties listed in the City of Lake Worth historic districts are recognized for their significance and are required to obtain a Certificate of Appropriateness (COA) when alterations, additions or renovations to the exterior of an existing structure are planned. New construction, relocations and demolitions within the historic district will be reviewed and must obtain a COA prior to beginning any construction. Interior renovations and ordinary maintenance are not reviewed. All larger projects involving extensive alterations, additions, demolitions or new construction are reviewed by the Historic Resources Preservation Board (HRPB), which has final authority, unless there is an appeal to the City Commission.

The criteria used to review COAs require construction to follow the U.S. Secretary of Interior Standards and the Historic Preservation Ordinance of Lake Worth. The intent is to ensure that the historical and architectural integrity of the building being reviewed is maintained and that new construction is compatible with the existing structures and surrounding context. Under the COA process, the applicant will post a notice at the premises of the proposed work in a location visible from the street within three working days of application submittal to the building department. Unless an applicant expressly waives an appeal, no COA will be effective for a period of fifteen days subsequent to the HRPB's decision. Appeals of staff decisions are sent to the HRPB and to City Commission for review of HRPB decisions.

## **HISTORIC RESOURCES PRESERVATION BOARD**

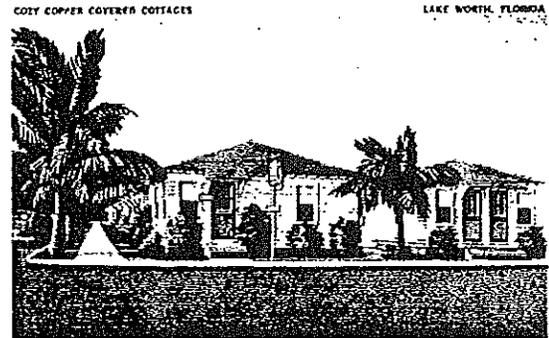
The Lake Worth Historic Resource Preservation Board was created by Ordinance, Chapter 23, and Article 27, of the Zoning Code. The City Commission appoints the nine members of the HRPB. They shall be complementary to the State Historic Preservation Officer, and have the following purposes:

- Affect and accomplish the preservation and protection of historic properties;
- Promote educational, cultural and welfare of people to safeguard the City's history;
- Stabilize property values;
- Foster civic pride and cultural stability through conservation of historic districts;
- Contribute to the City's economy through preservation and revitalization of historic resources;
- Protect character and scale of districts and protect against destructive encroachment;
- Provide review process for continued preservation and appropriate development of new construction, additions to structures within historic districts;
- Avoid adverse effects on landmarks and historic districts through demolition or alteration.

## HISTORICAL OVERVIEW

In the 1890s Mr. And Mrs. Samuel James homesteaded parts of the area now known as Lake Worth. In 1892 this land was sold to the Palm Beach Farms Company. The town was platted in 1912 as Lucerne by the Palm Beach Farms Company and included approximately 300 blocks and 7,000 lots. In 1913 the company filed a new plat for the south part of town, equal in size to the original plat. The City of Lake Worth was originally known as Lucerne, but the name was changed when it was found in the process of establishing a Post Office that the town of Lucerne already existed elsewhere in Florida. The commercial area was centered on Lake Avenue and Dixie Highway. Residential development continued to the north and south of these areas.

Moderate growth took place until the end of World War I, followed by a period of rapid growth that lasted until 1925. The Florida Land Boom collapsed in 1926 and, although some building continued in Lake Worth, like most building in south Florida new developments were far from complete, and the City was far from being built out. The 1928 hurricane and the national economic depression in 1929 brought building in Lake Worth to a standstill until after World War II, when development recommenced.



Historic Postcard  
200 Block of Lake Avenue c. 1925

The architecture of the Old Town Historic District reflects these two distinct periods of development.

The architecture of the early development is evidenced in the abundance of two-story Mediterranean revival and mission style buildings, particularly popular in south Florida in the 1920s. With the second building boom, the majority of the buildings are single-story, slab-on-grade masonry construction.

### Florida Boom Period (1919-1929)

Florida experienced an unprecedented period of growth during the post World War I period, known as the era of the Great Florida Boom. Immediately upon the war's end, real estate activity picked up, soon rising to a frenzied pitch. Property values rose dramatically. In virtually every city and town new subdivisions were platted and lots sold and resold for quick profits. Bank deposits swelled and droves of real estate companies set up shop in many towns and cities. State and county road systems expanded rapidly. Southeast Florida, particularly Miami and Palm Beach, entertained the most conspicuous activity, but few communities in the state escaped the fever. In 1920 construction of U.S. Highway 1 began in the area and by 1925 U.S. Highway 1 stretched from Maine to Miami.

The air began to seep out of the speculative land bubble in 1925. In August of that year the Florida East Coast Railway announced an embargo on freight shipments to south Florida, where ports and rail terminals were clogged with unused building materials. Devastating hurricanes that hit southeast Florida in 1926 and 1928 killed thousands of people and provided a sad, closing chapter to an era of wild excesses, plunging the state into economic depression. Adding to the economic misery, an infestation of the Mediterranean fruit fly devastated groves throughout the state in 1928.

# Design Guidelines for Old Town Historic Districts



Historic Postcard

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### Depression and New Deal Period (1929-1940)

The economic decline that first struck Florida fell within three years upon the nation at large, descending in full measure after the 1929 Wall Street Crash. By 1933 approximately one out of four Floridians was receiving some type of public relief and assistance.



Historic Postcard  
City Hall (Auditorium)  
7 North Dixie Highway

As the decade wore on, relief measures expanded, mostly inspired by the New Deal administration of Franklin Delano Roosevelt. The Works Progress Administration (WPA) provided jobs for professional workers and laborers alike, often employing them to construct roads and buildings. As a result the nation, the state, and communities by the thousands obtained infrastructure improvements they might otherwise never have attempted for lack of vision or means. Lake Worth's former Municipal Auditorium (now City Hall) was constructed with funds from the Civil Works Administration and the Federal Emergency Relief Administration.

## ARCHITECTURAL STYLES IN THE OLD TOWN HISTORIC DISTRICT

### Commercial (1900-1940)

Design of commercial buildings in Florida mirrored national trends. During the mid-nineteenth century, commercial buildings as a distinct property type developed throughout the United States. They housed a variety of uses, such as offices, banks, hotels, and theaters, but most commonly functioned as retail stores.

Specific design constraints shaped commercial architecture in the United States. Most commercial buildings were concentrated in districts with high land values. Lot configuration, therefore, exerted great influence on the form and plan of commercial buildings. To exploit land value to the fullest, commercial buildings were

constructed in close proximity to one another and designed to cover most of the lot. The sidewalls of one commercial building often formed party walls with adjacent buildings.



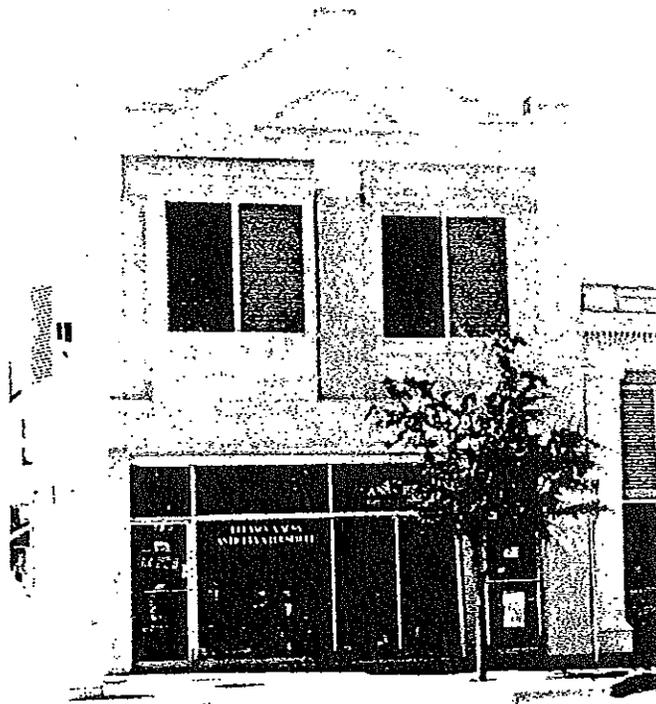
Historic Postcard  
Lake Avenue Commercial Buildings  
Looking East from Dixie Highway c. 1950

Because of such design constraints, commercial buildings from the mid-1850s to the 1940s shared many of the same characteristics. Most commercial buildings were rectangular in plan. One narrow elevation, facing the street, became the focus of the design and provided the building's identifying features. Facades were organized into distinct sections, or zones, commonly containing one or two parts.

The one-part facade generally was seen on one-story buildings and was formed by a structural framework consisting of columns, bulkheads or kick-panels, and a cornice topped by a parapet. Large, show windows were generally placed within this framework to display merchandise and light the interior. The wall area between windows and cornice provided a place for advertising and made the facade appear taller. This framework formed a basic compositional arrangement. Materials, doors and windows, and decorative and stylistic details constituted secondary characteristics that could be organized in a variety of ways.

The two-part commercial block was a multi-story building, organized into upper and lower zones. The design of the lower zone was essentially the same as the one-part facade. It

contained distinct uses in each zone. The lower zone generally housed public spaces such as retail stores, banking room, insurance offices, or hotel lobbies. The upper zone often provided space for private uses, including apartments, offices, hotel rooms, and meeting halls.



801 Lake Avenue c. 1920

Beginning in the 1920s two new masonry materials, hollow terra cotta tile and concrete block, gained wide use in construction of commercial buildings. As strong as fired brick, the new materials were lighter and cheaper. As the historic period drew to a close, concrete block replaced brick as a structural material. Beginning in the 1920s brick was frequently applied on a variety of commercial buildings as exterior finish material in combination with masonry or frame interior walls. Stucco finishes and terra cotta detailing became widespread, largely in association with Mediterranean stylistic influences. Construction of commercial buildings, along with all

other types of construction, declined in Florida during the 1930s. New styles such as Art Deco and Art Moderne became important influences on the design of commercial buildings.

**Characteristics:**

- Plan: regular, rectangular
- Foundation: continuous or slab brick or concrete
- Height: one to three stories
- Primary exterior material: brick, common or running bond; concrete block; stucco, rough texture

- Roof type: flat with parapet
- Roof surfacing: built-up
- Details: simple; usually cast concrete or ornamental brick such as corbeling; cast iron; terra cotta; vitrolite and carrera glass

### Frame Vernacular (1900-1940)



609 Lake Avenue c. 1912

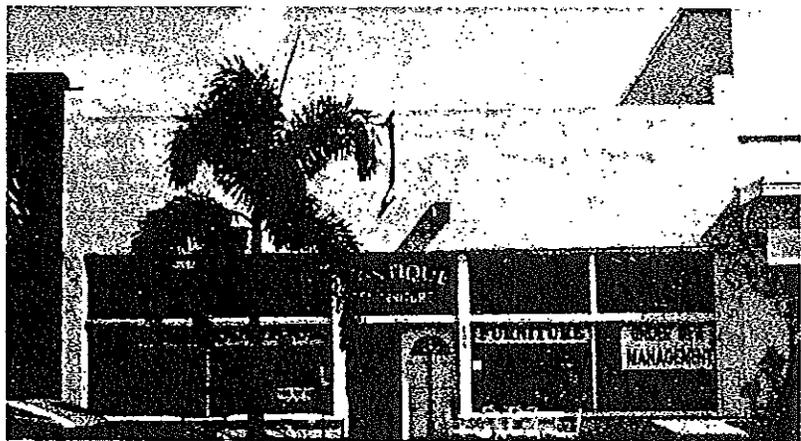
Vernacular architecture predominated in Florida from the Territorial Period until the Depression era of the 1930s. Frame vernacular architecture was the common wood-frame construction of self-taught builders, often passed from one generation to the next. Vernacular building traditions resulted from the builder's experience, available resources, and responses to the local environment. Most frame vernacular buildings were residential, but many churches and commercial buildings were also constructed.

Two buildings are identified as frame vernacular in the National Register of Historic Places application form. One is the building located at 609 Lake Avenue. This was Lake Worth's first Funeral Home; it then became a grocery store and later housed Kerr's Bicycle Shop from 1929 to the 1970s. This building was constructed circa 1912

and is the oldest building noted in the Historic District. The other frame vernacular building located in the District is the Central Market, a one-story building located at 806 Lake Avenue. Both of these buildings are of wood-frame construction.

#### **Characteristics:**

- Plan: regular, rectangular, ell and irregular' also common
- Foundation: Piers, brick and concrete block during 1920s
- Height: one to two-and-one-half stories
- Primary exterior material: horizontal wood siding
- Roof type: gable; less common hip, pyramidal, false front on commercial buildings
- Roof surfacing: metal, late 19<sup>th</sup> century; composition and asbestos shingles beginning in 1920s



806 Lake Avenue c. 1915

## Masonry Vernacular

(1900-1940)

Brick, the most common masonry material in the United States, was not readily available until the turn of the century in Florida because of poor transportation facilities and the indigenous soil content. Contractors for early brick structures in Florida, including fortifications, lighthouses, and arsenals, imported brick from other states for their works. As rail networks began to penetrate the Florida peninsula, brick became more readily available. Because of its fire-resistant qualities, brick was often used for commercial buildings.



728-730 Lake Avenue (1930)

After 1900, new colors and textures of brick were introduced. In addition to commercial buildings, brick was increasingly used on a variety of buildings, including private residences, apartments, schools, and governmental buildings. Beginning in the 1920s two new masonry materials, hollow tile and concrete block, became widely used. These new materials were as strong as fired brick, but were lighter and cheaper. In later years concrete block almost exclusively replaced brick as a structural material. During the 1920s brick was frequently used as a veneer in combination with masonry or frame interior walls on a variety of buildings.

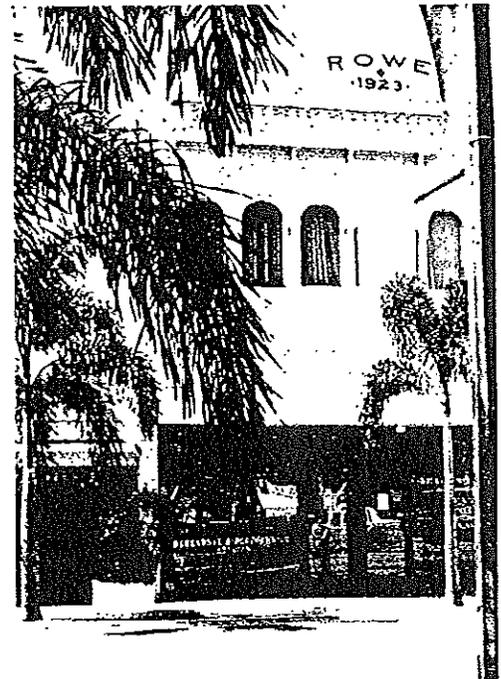
The exteriors are usually brick or stucco. The facade is typically symmetrical and the storefronts often contain full-glazed wood doors, transoms, wood kick-panels, and plate glass. Typical of commercial buildings is a two-story building in which the first floor is retail and the second floor designed to be used as office space or apartments.

The majority of the contributing buildings in Old Town Historic District are masonry vernacular. The structural systems may be of hollow clay tile, brick or concrete block.

Masonry vernacular buildings in downtown Lake Worth are the former Rose Apartments located at 728-730 Lake Avenue and The McCarthy Hotel located at 631 Lucerne Avenue. Both of the buildings have hollow clay tile structural systems.

The Rowe Building located at 811 Lake Avenue is listed as a masonry vernacular building, but also borrows other stylistic influences. While it has a hollow clay tile structural system, brick exterior and a flat roof, it also has classical Ionic columns adorning the upper facade in arched three-part windows. It also has brick in a tapestry design in the signboard area and barrel tile on the pent roof above the second-story windows.

### **Characteristics:**



811 Lake Avenue (1923)

- Plan: regular, rectangular
- Foundation: continuous or slab
- Height: one to two stories (apartments); one to two stories (commercial)
- Primary exterior material: brick, common or running bond; stucco, rough texture; concrete block, rusticated rock-faced
- Roof type: hip; flat with parapet
- Roof surfacing: composition shingles; built-up, commercial
- Ornamentation: simple; usually cast concrete or ornamental brick such as corbeling

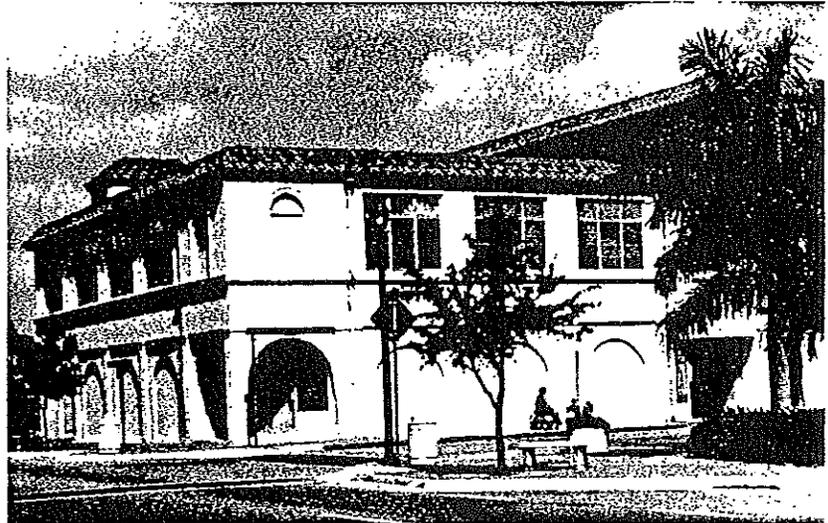
### **Mediterranean Influence (1885-1940)**

Spanish and other Mediterranean-influenced styles were most common in Florida, California and the Southwest. Spanish Revival architecture, popularized at the 1915 Panama-California International Exposition at San Diego, swept through California, the Southwest, and Florida within a few years.

Florida's Spanish heritage and semitropical climate favored use of Mediterranean designs. The roots of Mediterranean-influenced architecture in Florida can be traced to the Spanish, Italian Renaissance, and Moorish Revival hotels. During the great Florida land boom of the 1920s architects and builders applied Spanish, Spanish Colonial Revival, Mission, and other Mediterranean-influenced designs to a wide spectrum of buildings.

Although the term "Mediterranean Revival" is indiscriminately applied to all buildings with features derived from Mediterranean architecture, many, particularly those designed by architects, were consciously modeled on formal styles.

Although modified repeatedly over the years, one example of the Mediterranean Revival style is the former Scottish Rite Temple located at 1000 Lake Avenue. Damage



1000 Lake Avenue c. 1921

from the 1928 hurricane and numerous remodels have resulted in the loss of many of the original architectural details. There was originally a four-story square tower on the corner. This two-story building does exhibit a hollow clay tile structural system, stucco exterior, and a flat roof accented with barrel tile. Square pilasters support repeating arches on the first floor. Numerous remodels have altered the original style.

The most commanding building in the Old Town Historic District by size, site location and architectural character is the Municipal Auditorium. Currently used as City Hall, the building that was constructed from 1933 to 1935, is a fine example of Moorish Revival. The architect was the well-known G. Sherman Childs. This is a two-story stucco covered structure, resting on a half-exposed rusticated basement. Pilasters are seen on the north and south façades with stucco quoins at the corners. There is a barrel-vaulted roof with ornamented parapet and spires.



**Characteristics:**

- Plan: irregular
- Foundation: continuous
- Height: two stories
- Primary exterior material: stucco
- Roof type: combinations of hip and flat, Mission roof: flat with parapet curvilinear
- Roof surfacing: barrel
- French interlocking tile, built up on flat roofs
- Detailing: plaster and terra cotta detailing highlighting arches, columns, window surrounds, niches, cornices, and parapets; wrought iron grilles, tile roof & decorative brackets, balconies and balconets

Historic Postcard  
7 North Dixie Highway c. 1933

**Classical Revival  
(1900-1930)**

Classical Revival, also known as Neoclassical, resulted from an adaptation of the Greek temple front and other details to a variety of structures. The Classical Revival provided a more subdued alternative to the Beaux Arts, which featured ostentatious, sculptured ornament and highly decorated moldings. Classical Revival was frequently associated with major public buildings and private residences designed by formally trained architects.

In Florida the Classical Revival was found on a variety of building types. Although scattered examples of the style in Florida date to the 1890s, it did not become common until the following decade. Many of the earliest examples consisted of large private residences and estates.

Whitehall, in Palm Beach, designed in 1901 by the New York firm of Carrere and Hastings as a winter home for Henry Flagler, provides a most notable example. Over the next several decades the Classical Revival exerted a major influence on the design of public buildings such as courthouses and commercial buildings, particularly banks.

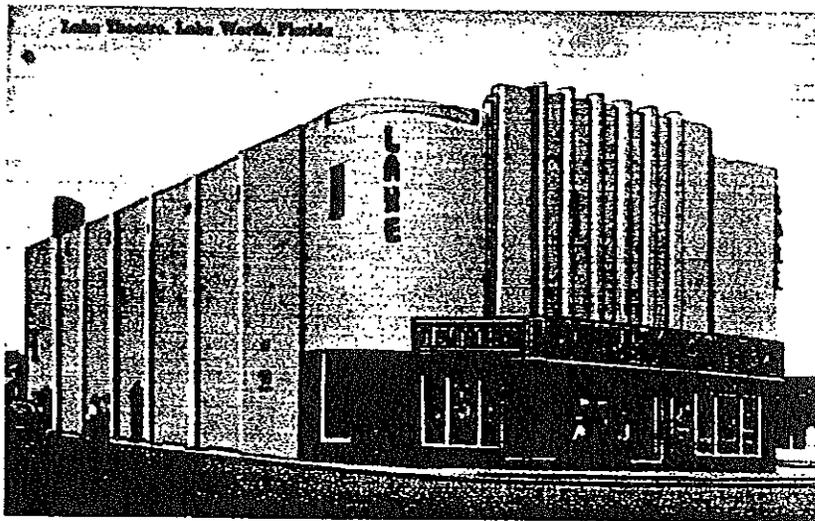
The former Bank of Lake Worth is the only Neo-classical Revival building in the Old Town Historic District. This building has a symmetrical temple front, with full-height pilasters. Dentils decorate the cornice and entry. The main entrance opens at the center of the facade. The exterior is smooth masonry.



803 Lake Avenue c. 1919

**Characteristics:**

- Plan: regular, rectangular or nearly square
- Foundation: piers or continuous, brick or concrete
- Height: two to two-and-one-half stories
- Primary exterior material: horizontal wood siding; smooth masonry
- Roof type: low-pitched hip or flat with a balustrade
- Roof surfacing: embossed sheet metal or metal shingles; composition, asbestos shingles; built-up on flat roofs
- Detailing: classically derived; full-facade height Ionic columns, balustrades, medallions, dentils
- Entrance detailing: transom, sidelights, ornamental woodwork common



Historic Postcard  
Lake Theatre

**Art Deco – Art Moderne  
(1925-1940)**

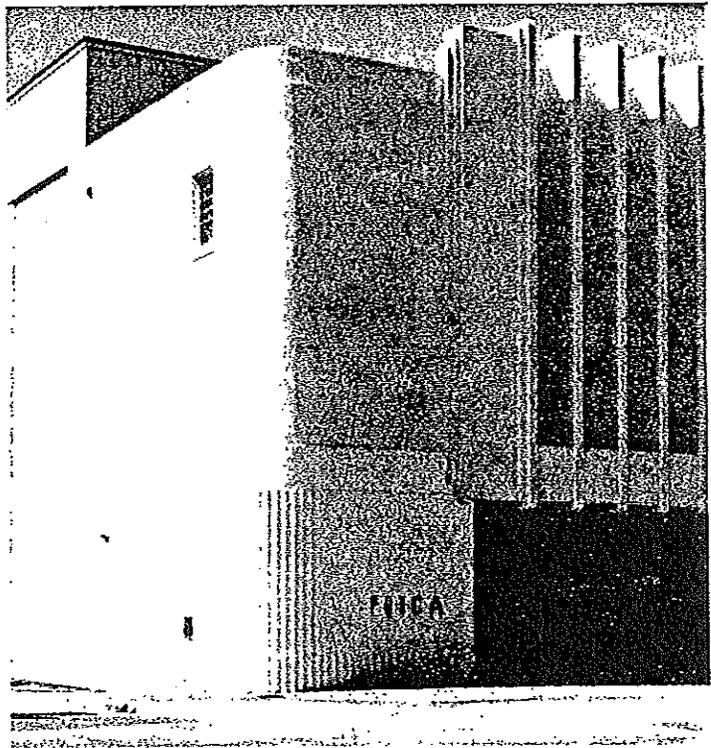
Art Deco, the fashionable style of the 1930s, influenced arts and crafts, sculpture, and painting, as well as architecture. It represented a dramatic departure from traditional design, looking to the future rather than the past. The style derived its name from the Exposition Internationale des Arts Decoratifs and Industriels Modernes, a 1925 Paris showcase for new artistic designs.

Ornament defined Art Deco design, incorporating stylized floral patterns and repetitive geometric forms employing sharp angles and segments of circles. The style's features particularly suited it to tall buildings, resulting in its popularity during the 1920s and early 1930s.

In Florida, Art Deco was widely applied to commercial and apartment buildings, notably in communities that continued to grow despite the economic depression that enveloped the state from 1926 to the beginning of World War II. Miami and Miami Beach contain the best collection of Art Deco commercial and apartment buildings in America. After 1930 the related Art Moderne style became the more popular of the modernistic styles.

Characteristics of the Art Deco style included a flat roof, irregular plan, stucco exterior finish, and low relief, polychromatic ornamentation in straight line, zigzag, geometric floral, and chevron designs. In Europe the ornamentation was influenced by cubism, while in the United States the designs were based on North and South American Indian Art.

The former Lake Theatre, located at 601 Lake Avenue, is a very good example of Art Moderne. This is a two-story building with a flat roof, curved corners and smooth stucco walls. Vertical emphasis is given by full-height vertical elements that occur on the sides of the building. The front facade has strong vertical elements and a centrally located entrance. The building originally had a large marquee that has subsequently been removed. The building was later known as the Worth Theater and is now home to the Palm Beach Institute of Contemporary Arts.



601 Lake Avenue (1939)

**Characteristics:**

- Plan: irregular
- Foundation: continuous
- Height: two to three stories
- Primary exterior material: concrete, stucco, terra cotta, and glass
- Roof type: flat, with parapet
- Roof surfacing: built-up
- Detailing: zigzags and other geometric and stylized motifs; towers and other vertical projections

## **DESIGN GUIDELINES FOR THE OLD TOWN HISTORIC DISTRICT**

The Old Town Historic District Guidelines apply to the area adopted as the Old Town Local Register Historic District as adopted by the City Commission on June 11, 1999 (see Map 2). The area of the Local District encompasses the entire area of the Old Town National Register Historic District as well. These guidelines are designed to preserve the historic, cultural and architectural character of the Historic District. In case of conflict with other requirements, the most stringent requirement shall apply.

### **OLD TOWN HISTORIC DISTRICT MATERIALS PALETTE**

Materials are an important part of the fabric of any building or historic district. Significant materials should be identified before undertaking the rehabilitation of a building or other historic property. Lake Worth's Old Town Historic District does not exhibit a wide variety of materials. Significant is the number of stucco buildings. This section contains descriptions of the significant materials found in the Historic District.

#### **Masonry**

Brick was rarely used in Florida. Clay was not indigenous to the state, and a primitive transportation system made this a scarce material. At the periods of development in Old Town, "masonry" usually refers to concrete block or hollow clay tile. Concrete block or hollow clay tile units were then typically covered with stucco.

However, brick is significant material in Old Town because of the scale and color and, if originally exposed, should be retained. Brick must breathe, and exposed brick should not be painted. Brick patterns are seen in The Rowe Building, constructed in 1923, which exemplifies the use of Flemish Bond with Herring Bone Detail.

#### **Stucco**

Stucco, an exterior wall covering, consists of a mixture of portland cement, sand, lime, and water. Sometimes crushed stone or shell is added for texture.

In Lake Worth, stucco is typically applied with coarse finishes. Other finishes include smooth, pebble dash, shell dash and dry dash. Stucco finishes were associated with a variety of building styles and building types. These included the Art Deco, Art Moderne, and many Mediterranean Revival styles, among them the Mission. Resort hotels, apartment buildings and movie theatres were among the building types typically finished in stucco.

In Florida, stucco gained popularity during the Great Boom of the 1920s, usually in association with revival styles, especially Mediterranean Revival. It was also applied to existing buildings, particularly brick commercial structures to give them a contemporary look. Matching the texture and finish of the stucco is extremely important and will require some samples to be prepared prior to initiating stucco repairs or additional new work to an existing stucco building.

#### **Wood**

Wood has been the most common construction material in Florida since Colonial times. Carpenters and sawmill operators produced structural members, exterior cladding, and shingles from indigenous woods such as heart pine, red cedar, and cypress. During the mid-nineteenth

century, as transportation expanded and the production of building materials became industrialized, milled lumber and other wooden construction elements proliferated.

During the early twentieth century, wood remained an important building material in Florida. In the commercial areas, wood is primarily used for structural members; however, lap siding is still visible in the Historic District at 609 Lake Avenue, along the building's side elevation.

### Roofing Materials

In the Old Town Historic District, roofs are generally flat, built-up roofs; however, projecting pent roof of clay barrel tile or projecting overhangs can be a significant characteristic of the building.

Care must be taken to protect and maintain these attributes.

The materials used for the roofs of buildings throughout the district vary. Clay tile is often seen on Mediterranean-style buildings as well as others in the district. With visible roofs, such as on free-



Historic Postcard  
Lake Avenue c. 1930's  
Looking southeast from Dixie Highway

standing buildings such as City Hall and the Library, the clay tile roofs are significant architectural elements of the building.

It is important to repair or replace roofing with materials similar to the original in size, color and texture. Tiles roofs are the most important to retain because of the color, texture and stylistic qualities they provide for a structure. The Mediterranean-style buildings, with their low-pitched tile roofs and decorative brackets that support the roofs, are an important contributing factor to the Historic District.

## **TYPES OF REHABILITATION FOR HISTORIC BUILDINGS**

This section addresses rehabilitation of historic buildings and their sites. It begins with definitions of the major approaches to altering or repairing a historic building. Following are definitions and approaches to be used to develop a rehabilitation plan for buildings within the Old Town Historic District.

### **Remodeling**

Remodeling is an approach in which repairs or alterations are undertaken with little regard for the overall design and individual features of a historic building. During the course of remodeling, the historic character of a building is usually lost. Remodeling is not a recommended approach in a historic district. It will frequently result in rejection of a certificate of appropriateness and denial of tax credits for revenue-producing buildings and ad valorem tax exemptions.

### **Stabilization**

Stabilization, usually the first step in preserving a historic building, is undertaken to re-establish the weathertight quality and structural integrity of buildings. It is a temporary measure designed to allow rehabilitation or restoration in the future. Stabilization measures include repairing or covering roofs and windows so that rain cannot penetrate the interior, extermination of insects, protecting a property from vandalism, and other work that will prevent further deterioration. This is particularly useful for larger public buildings that require time to locate an appropriate tenant.

### **Restoration**

Restoration is accurately recovering the form and detail of a building and its setting as it appeared at a specific time in the past. Restoration often may require the removal of later work or the replacement of missing earlier work. Restoration is the most accurate and expensive means of preserving a building. Because of the cost, restoration is generally employed only on landmark buildings. Restoration entails research into the history, development, and physical form of a building and attention to detail. The original use is generally maintained or interpreted.

### **Reconstruction**

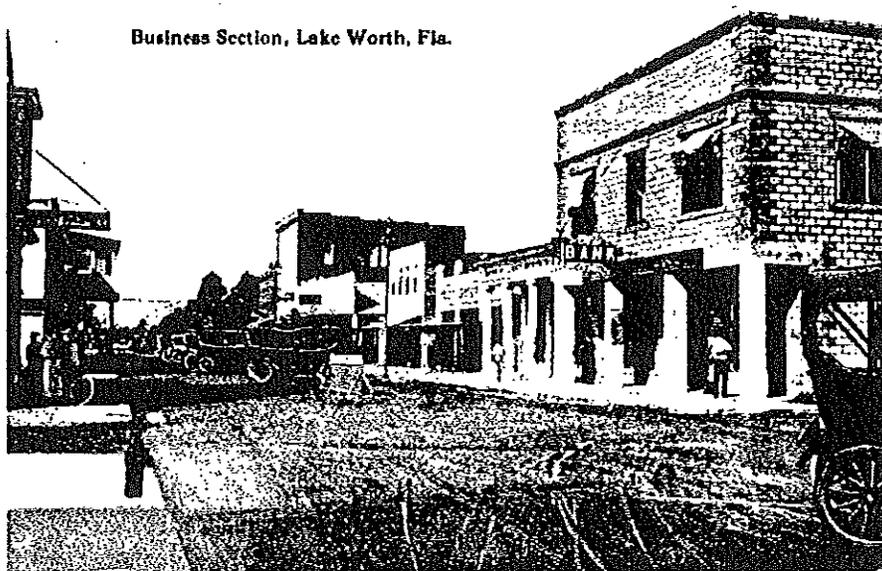
Reconstruction entails reproducing, by new construction, the exact form and detail of a vanished building or part of a building, to its appearance during a specific time in its history. Reconstruction is recommended only when there is adequate historical, pictorial, or physical documentation so that a building or feature can be accurately reproduced. Conjectural reconstruction is not a recommended approach and conflicts with contemporary preservation standards.

### **Rehabilitation**

Rehabilitation is a practical approach to historic preservation. It is the process of repairing or altering a historic building for an efficient contemporary use while retaining its historic features and character. Rehabilitation represents a compromise between remodeling, which has no sensitivity to the historic features of a building, and restoration, which is a highly researched and extremely accurate method. Rehabilitation is costly and its most important use is to educate the public about architecture and historic preservation in highly visited properties.

Rehabilitation frequently involves changes in use or adaptive reuses. It includes structural repairs, repairing roofs and exterior finishes, painting, and upgrading mechanical systems. These changes may result in physical alterations, such as additions, expanded parking, and measures to comply with contemporary health and safety code requirements. Sensitive rehabilitation results in changes that do not negatively impact the historic character of a building and its setting.

## GUIDELINES FOR REHABILITATING HISTORIC PROPERTIES



Historic Postcard  
Lake Avenue looking west c. 1919

The guidelines that follow are oriented toward rehabilitation of historic buildings and other historic properties. They essentially draw upon the Secretary of the Interior's Standards for Rehabilitation. The Secretary of the Interior's Standards are the authoritative guidelines for rehabilitation in the United States. The Lake Worth Historic Preservation Ordinance incorporates the Secretary of the Interior's Standards as a basis of review.

The Standards suggest a series of steps to rehabilitation, beginning with the least intrusive treatments. The steps in sequence are as follows:

### 1. Identify, Retain, and Preserve

Identifying, retaining, and preserving the form and detailing of architectural materials and features are basic to the sensitive treatment of all historic buildings. The guidelines recommend measures to accomplish this goal while avoiding actions that will cause the removal of features that form the historic character of a building.

### 2. Protect and Maintain

Protection involves the least degree of intervention and precedes other work. Protective measures include the maintenance of historical materials through treatments such as caulking, application of protective coatings, and cleaning of roof gutter systems; or stabilization and protection of the site. Although a historic building will usually require more extensive work, an overall evaluation of its physical condition should begin at this level.

### 3. Repair

Repairs are warranted when the physical condition of character-defining materials and features require it. Repair of historic material begins with the least degree of intervention possible: patching, splicing, consolidating, or reinforcing the material according to recognized preservation methods. Repair includes limited replacement in kind or with a compatible substitute material for extensively deteriorated or missing parts of features when there are surviving prototypes. Although using the same kind of materials is preferred, substitute materials are acceptable if the form and design as well as the substitute materials themselves convey the visual appearance of the remaining parts of the feature and finish.

**4. Replace**

Replacement is appropriate when an entire character-defining feature cannot be repaired. If the form is still evident, the physical evidence can be used to re-establish the feature as an integral part of the rehabilitation project. Like the guidance for repair, the preferred option is always replacement of the feature with the same material.

**5. Alterations/Additions to Historic Buildings**

The final step involves alterations and additions. Some alterations to a historic building are generally needed to assure its continued use. It is, however, important that such alterations do not radically change, obscure, or destroy character-defining spaces, materials, features or finishes. Alterations may include new entrances or windows on secondary elevations and installing mechanical systems. Alterations may include the selective removal of building or other features that are intrusive and therefore detract from the overall historic character.

The construction of an addition to a historic building is allowable and should be carefully planned to be appropriate to the historic building. Mimicking or cloning the historic building is not required. Creating an appropriate addition requires consideration of massing, material and design scale.

**Uncovering Original Detail**

In approaching a rehabilitation project, every effort should be made to uncover any previously encased or hidden finishes and details such as siding, stone, ornamental plaster and decorative elements. It is often the case that the "sealing" of these items in an enclosure of metal or other material has created a poorly ventilated condition, which leads to deterioration. In addition, the covering of original materials on these historic buildings detracts from the building's historic value.

**Recommendations:**

- *Uncover original detail that has been covered by later changes to the building and repair these details, if necessary.*
- *Uncover original wall materials that have been covered during later modifications by aluminum, vinyl or asbestos siding.*

**Avoid:**

- *Covering any original detail with new construction.*
- *Sandblasting to remove paint, stucco or plaster.*
- *Using flame to remove paint from wood surfaces.*

**Maintaining Original Building Materials**

The key to a successful rehabilitation is maintaining the architectural character and historic fabric. If replacement of existing materials is essential, materials similar in proportion and style to the original materials should be used.

When introducing new elements to the exterior, materials similar in proportion and detail to the characteristic style of the particular building should be used. This alters both the building and the streetscape. Introduction of new entries and changing the original "storefront" must be avoided. Retain original solid-void relationship; retain or restore ground floor facades, especially in the commercial downtown.

### Maintaining Original Building Materials

**Recommendations:**

- Replace deteriorated materials and details with materials that match as closely as possible the proportion and detail of the original material.

**Avoid:**

- Replacing original details with new details, which do not match the original in scale, proportion, or detail.
- Adding uncharacteristic materials to the building. For example, aluminum siding, vinyl siding, metal-frame windows, etc.

### Storefronts, Doors and Entrances

Storefronts frequently define the historic character of commercial buildings in Florida. Entrances, display windows, trim, cornices, and decorative detailing are particularly important. Placement of entrances and windows can create a distinct rhythm on the facade of a building. When rehabilitating a storefront, such features, materials, and design elements should be retained and repaired.



Storefront at 611 Lake Avenue

Unfortunately, storefronts have been particularly subject to alteration. This was especially true in Florida cities during the 1950s and 1960s, when rapid growth and economic prosperity led to frequent re-modeling or removal of historic storefronts. Under these circumstances, two options are available when planning rehabilitation. Where original or early storefronts no longer exist or are too deteriorated to save, two options are appropriate. The first option is to retain the commercial character of the building through contemporary design. The new design should be compatible with the scale, design, materials, color and texture of the historic building. The second option is to restore the storefront based on historical research and physical evidence.

Principal doors and entrances are an integral part of historic buildings in Florida. They frequently contain decorative or stylistic features, such as transom and sidelights or detailed surrounds. Doors and entrances and associated detailing should be preserved. Changes to door size and configuration should be avoided.

Replacement doors should either match the original or substitute new materials and designs sympathetic to the original. Historic doors that do not match the composition and stylistic details of the building or missing door should not be substituted. Any ornamentation should be based on historic precedent and in keeping with the character of the door and entrance design.

Placement of new entrances on principal facades should be avoided. New entrances can result in loss of historic fabric and detailing and change the rhythm of bays. New entrances should be compatible with the building and be located on walls on the side of the building that are not readily visible from the public right-of-way.

### Storefronts, Doors and Entrances

<b>Recommendations:</b>
<ul style="list-style-type: none"> <li>• Retain and repair existing storefronts, including windows, sash, doors, transoms, signage and decorative features where such features contribute to the architectural and historic character of the building.</li> </ul>
<ul style="list-style-type: none"> <li>• Evaluate the overall condition of storefront materials to determine whether more than protection and maintenance are required.</li> </ul>
<ul style="list-style-type: none"> <li>• Where original or early storefronts no longer exist or are too deteriorated to save, retain the commercial character of the building through contemporary design, which is compatible with the scale, design, materials, color and texture of the historic buildings; or an accurate restoration of the storefront based on historical research and physical evidence.</li> </ul>
<ul style="list-style-type: none"> <li>• Retain and repair historic door openings, trim, and details such as transom, sidelights and hardware where they contribute to the architectural character of the building.</li> </ul>
<ul style="list-style-type: none"> <li>• Protect and maintain the materials that comprise entrances.</li> </ul>
<ul style="list-style-type: none"> <li>• Replace missing or deteriorated doors with doors that closely match the original design.</li> </ul>
<ul style="list-style-type: none"> <li>• Place new entrances on secondary elevations away from the main elevation. Preserve nonfunctional entrances that are architecturally significant.</li> </ul>

<b>Avoid:</b>
<ul style="list-style-type: none"> <li>• Removing or radically changing storefronts and their features that are important in defining the overall historic character of the building so that as a result, the character is diminished.</li> </ul>
<ul style="list-style-type: none"> <li>• Introducing a storefront or new design element on the ground floor, such as an arcade, which alters the architectural and historic character of the building and its relationship with the street or its setting or which causes destruction of significant historic fabric.</li> </ul>
<ul style="list-style-type: none"> <li>• Using materials, which detract from the historic or architectural character of a building.</li> </ul>
<ul style="list-style-type: none"> <li>• Introducing a new design that is incompatible in size, scale, material and color.</li> </ul>
<ul style="list-style-type: none"> <li>• Introducing or changing the location of doors and entrances that alter the architectural character of the building.</li> </ul>
<ul style="list-style-type: none"> <li>• Stripping entrances of historic material such as wood, cast iron, terra cotta tile and brick.</li> </ul>
<ul style="list-style-type: none"> <li>• Replacing deteriorated or missing doors with stock doors or doors of inappropriate designs or constructed of inappropriate materials.</li> </ul>
<ul style="list-style-type: none"> <li>• Installing secondary service entrances that are incompatible in size and scale with the historic building or obscure, damage or destroy character-defining features.</li> </ul>
<ul style="list-style-type: none"> <li>• Aluminum, metal, and jalousie doors should be avoided.</li> </ul>

### Awnings

Awnings were sometimes featured on buildings in Florida. They are functional, decorative, and appropriate to the many historic buildings, particularly Mediterranean-style buildings. These awnings were typically made of canvas in the 1920s, but during the second phase of development in Lake Worth, decorative metal and aluminum awnings were used.

Replace awnings to match originals, or if installing new awnings, they can be of compatible contemporary design. They should follow the lines of the window openings. Round-shaped are appropriate only for Mediterranean-styled buildings. Angled, rectangular canvas awnings are most appropriate for flat-headed windows and storefronts. Awnings that obscure significant detailing are inappropriate.

**Appropriate:**

- *Install awnings that are historically appropriate to the style of the building or that are of compatible contemporary design.*
- *Awnings should follow the lines of window or door opening they are intended to cover.*



Various Awnings and Overhang  
South side of 600 block Lake Avenue

**Inappropriate:**

- *Installing on significant facade shutters, screens, blinds, security grills and awnings that are historically inappropriate and detract from the building's character.*
- *Installing metal awnings on buildings that did not originally have metal awnings.*
- *Installing awnings that obscure architecturally significant detailing or features.*
- *Replacing architecturally significant detailing, such as commercial canopies, with awnings.*
- *Awnings/awning colors used as corporate logo.*

### Painting/Colors

Paint colors, finishes and decorative painting constitute important factors in defining the character of a historic building. Because of frequent painting, few buildings in Florida exhibit original colors; the best way to verify original colors is through a scientific paint analysis.

**All buildings in the Old Town Historic District are subject to the City's paint approval permitting process. This Code limits trim colors to two and base color to one.**

#### Recommendations:

- *Preserve painted and unpainted surfaces, as they traditionally existed on a building.*
- *Apply compatible paint coating systems following proper surface preparation.*
- *Choose color appropriate to the period and style of the building and district.*
- *In the absence of historic information on paint colors, use low reflective, subtle neutral pastels, white or earth colors.*

#### Avoid:

- *Using methods of removing paint, which are destructive, such as sandblasting, application of caustic solutions, or high-pressure water blasting.*
- *Painting a traditionally unpainted surface and removing paint from a traditionally painted surface.*
- *Creating a new appearance by applying paint or other coatings such as stucco to masonry that has been historically unpainted.*
- *Removing paint from historically painted masonry.*
- *Radically changing the type of paint or coatings or its color.*
- *Color used as a corporate logo, high-intensity, black, or metallic colors.*
- *Painting large murals or trompe l'oeil on surfaces that were traditionally unembellished.*

### Parking and Access

In the Old Town Historic District the traditional place for parking is on the streets and behind the commercial buildings. It is important that the historical character of the continuous façade is maintained along all streets and that entrances to parking are located along the alleys that parallel the streets.

#### Recommendations:

- *Use existing alleys to provide access to buildings.*
- *Limit parking to the rear of buildings, unless it was historically located in other areas.*

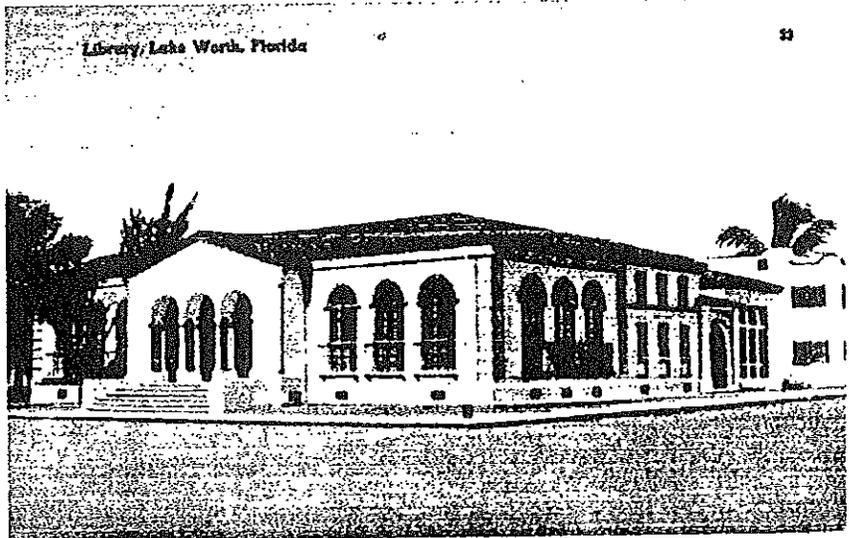
#### Avoid:

- *New curb cuts and driveways that break the solid street edge.*
- *Parking on the front side of buildings.*
- *Drive throughs*

### Roofs and Roofing

Often, the architectural character of an older building is expressed most in its roof form and roofing material. Most of the roofs in Downtown Lake Worth are gabled, hipped or flat. The roofs overhang the building walls to protect the window and door openings and to provide shade. These eaves are sometimes enclosed and, in many cases, embellished with wooden brackets. In other cases, rafter tails (the ends of the structural members of the roof) are exposed rather than covered by soffits.

Wood fascia boards (a horizontal board between the edge of the roof and the soffit) are also common among the various architectural styles.



Historic Postcard  
Lake Worth Library c. 1941  
15 North M Street

Each of these elements is important in defining the character of the building. Every effort should be made to retain these features or repair them. Metal or synthetic soffits and fascias are not compatible with the materials characteristic of Old Town.

#### **Recommendations:**

- *Maintain the original roofline.*
- *Maintain the original roofing material.*
- *Repair or replace roofing with material as close to the original as possible in size, shape, color and texture.*

#### **Avoid:**

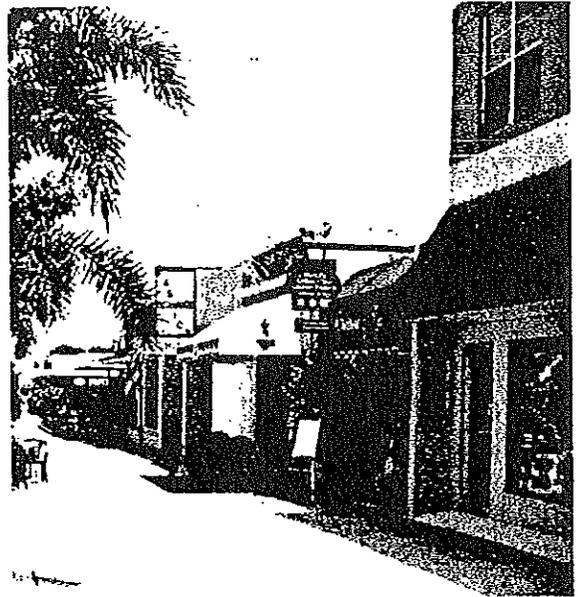
- *Altering the original roofline.*
- *Replacing the roofing with a material that is not characteristic of the building's style.*
- *Replacing soffits with metal or synthetic material.*

### Signs

Signs are an important component of commercial architecture. Their purpose is to provide information about the location and type of business housed in a building. Traditionally, a variety of types of signs have been associated with commercial buildings. These include fascia signs, placed on the fascia or horizontal band between storefront and second floor; hanging, projecting signs, which extend from a building; gold leaf signs, which are painted or etched in glass in windows, doors, and transoms; awnings or canopies on which signs are painted; and, beginning in the 1920s, neon signs which were illuminated by electricity and appeared in a variety of shapes, colors and images. The historic Oakley Theater (later known as the Worth Theater) at 713 Lake Avenue originally had a projecting marquee marking its entry, advertising the show and protecting its patrons from the elements.

In some instances signs were fully integrated into the overall design and style of a building. Some of the best examples are drawn from the Art Deco and Art Moderne styles that were common in Florida during the 1930s. When signs are a significant historic feature of a building, they should be respected and, in some instances, signs that were later additions such as neon signs or theater marquees might have achieved significance in their own right.

New signs should be compatible with the architectural character of a building. Factors to consider in selecting a sign are its legibility, clarity, placement, durability and appropriateness to the size and scale of building. Appropriate locations are the flat, unadorned parts of a facade such as the glass of storefronts, awning flaps, masonry surfaces and cornice frieze panel.



Typical Historic District Signage  
South side 500 block of Lake Avenue

Signs should not obscure architectural detailing such as windows, cornice details or storefronts and should not interfere with the view of the facades of adjoining buildings. Sign panels should be square or rectangular and flush-mounted. Block-style lettering is most appropriate. Large signs are appropriate for highway strip development where customers pass businesses at high rates of speed. They are inappropriate for historic buildings, where traffic flow is slower and the orientation and setback of buildings make them difficult to read.

Article XXVIII of the Lake Worth Code has requirements regarding maximum size of signs and spacing of signs, which also apply to buildings in the Old Town Historic District.

### Signs

#### Recommendations:

- Retain historic signs which are associated with historic figures, events, and places; evidence history of product, business, service advertised; reflect history of a building or development of the historic district; contain significant materials such as gold leaf, neon, or stainless steel; are integral to a building's design or physical fabric as when a historic name or date are rendered in stone, metal, or tile; are outstanding examples of a sign maker's craft; or are local landmarks recognized as focal points of a community.
- Locate new sign on the flat, unadorned parts of a facade, such as show windows, awning flaps, fascia and frieze, or other areas of building where signs have traditionally been placed.
- Use simple designs and lettering such as blockstyle and serif style, painted in high contrast to the sign panel color.
- Sign panels should be square or rectangular and flush-mounted.

#### Avoid:

- Removal of historically or architecturally significant signs.
- Ornate signs or signs based on architectural styles inappropriate to the commercial architecture of a district.
- Signs that obscure architectural details such as windows, cornice, decorative brickwork and storefronts.
- Signs that interfere with sight lines of adjoining buildings.

### Windows/Shutters

The placement, design, and materials of windows are often a significant part of the architectural character of a building. Common historic windows in Lake Worth are wood, double-hung sash; wood or steel casement types; along with commercial storefront windows. Windows often offer or contain significant stylistic elements.

The visual role of historic window design in the overall elevation should be carefully considered in planning window repair or replacement. Factors to consider include the size and number of historic windows in relation to a wall surface and their pattern of repetition; their overall design and detailing; their proximity to ground level and key entrances; and their visibility, particularly on key elevations.



Typical Window Fenestration in Old Town

Windows that are a significant part of the overall design of a building should not be destroyed or replaced. Careful repair is always the preferred approach. However, in the case of many storefront windows, the original windows have often been altered or removed. In this case, replacement of windows to match the original is highly recommended. New windows, which match the original in size, style and materials, may be substituted for missing or irreparable windows.

Owners often wish to replace windows to create a new look; however, this is not an appropriate choice within a historic district. Different-sized windows, highly tinted windows, windows with reflective qualities, or windows of incompatible design and materials are not appropriate rehabilitation.

<b>Recommendations:</b>
<ul style="list-style-type: none"> <li>• Retain and repair window openings, frames, sash, glass, lintels, sills and awnings where they contribute to the architectural and historic character of the building.</li> </ul>
<ul style="list-style-type: none"> <li>• Research historical evidence of original windows.</li> </ul>
<ul style="list-style-type: none"> <li>• Conduct an in-depth survey of the conditions of existing windows early in rehabilitation planning so that repair and upgrading methods and possible replacement options can be fully explored.</li> </ul>
<ul style="list-style-type: none"> <li>• Protect and maintain the wood and architectural metal which comprise the window frame, sash, muntins, and surrounds through appropriate surface treatments such as cleaning, rust removal, limited paint removal, and re-application of protective coating systems.</li> </ul>
<ul style="list-style-type: none"> <li>• Improve the thermal performance of existing windows and doors through adding or replacing weather-stripping.</li> </ul>
<ul style="list-style-type: none"> <li>• Designing and installing new windows when the historic windows are completely missing. The replacement windows may be an accurate restoration using historical, pictorial, and physical documentation; or be a new design that is compatible with the window openings and the historic character of the building.</li> </ul>

<b>Avoid:</b>
<ul style="list-style-type: none"> <li>• Changing the location or size of windows and other openings that alter the architectural and historic character of a building.</li> </ul>
<ul style="list-style-type: none"> <li>• Changing the size or arrangement or windowpanes, muntins, and rails where they contribute to the architectural and historic character of a building.</li> </ul>
<ul style="list-style-type: none"> <li>• Replacing windows that contribute to the character of a building with those that are incompatible in size, configuration, and reflective qualities or which alter the setback relationship between window and wall.</li> </ul>

## Guidelines for Additions

Additions to historic buildings are often required for practical reasons. Although additions are usually acceptable, they should be undertaken only after it has been determined that the new use cannot be successfully met by altering noncharacter-defining interior spaces. If undertaken, additions should not significantly alter original distinguishing qualities of building such as the basic form, materials and fenestration. The Department of the Interiors Standards for Rehabilitation requires that additions to historic properties be compatible with the historic character of the building and site. However, it is important that a false historic appearance be avoided that does not allow the actual historic structure to be identified.

Additions should result in minimal damage to the original building and its character-defining features. Character-defining features of a historic building should not be radically changed, obscured, damaged or destroyed in the process of adding new construction. The size and scale of the new addition should be in proportion to the historic portion of a building and clearly subordinate to it. Additions should be attached to the rear or least conspicuous side of a building.

## Guidelines for New Construction and Demolition

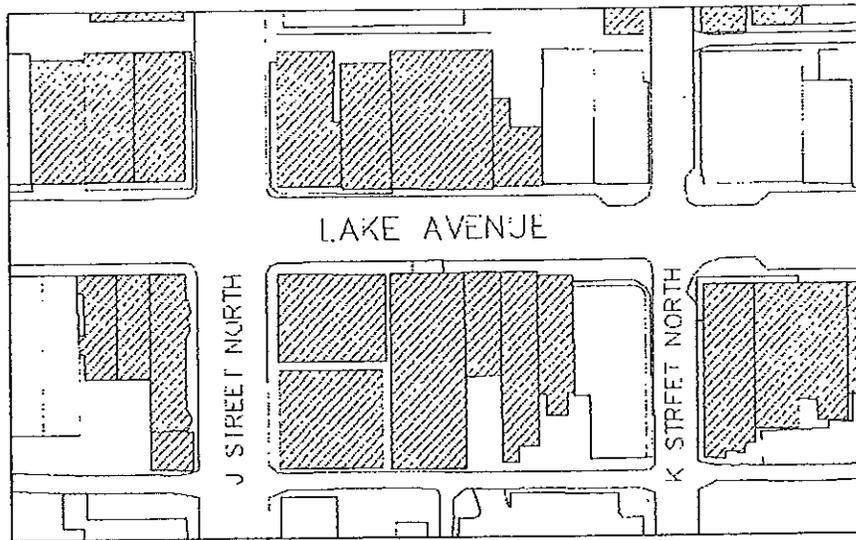
Design guidelines focus attention on those special visual and special qualities that a historic district is established to protect. Since architectural styles and details vary within the district, the HRPB will review new projects on an individual basis in terms of immediate context and overall character of the district.

### **Recommendations:**

- *Setback. Maintain the historic building setback that occurs in buildings in the surrounding blocks. See illustration on the following page.*
- *Orientation and site coverage. The front of the new building is in relationship to the front of other buildings along a block. Side setbacks are typically zero.*
- *Maintaining materials within the district. Use materials common to the district and avoid the use of inappropriate materials in construction. See the Old Town Historic District Materials Palette.*
- *Trim and detail. New construction should be compatible with historic buildings without necessarily copying their detail.*
- *Entrances, overhangs, canopies and cornices. Maintain size, shape and proportion of entrances and details, especially on the street level.*
- *Roof forms. Identify roof forms that are consistent with the building type used and complementary in color and scale to roofs in the historic district.*
- *Signage and lighting. Use sign materials and lighting types that are typical to historic buildings in the district.*

### **Avoid:**

- *Introducing new structures or site features that are out-of-scale to the historic building and site.*
- *Creating false historic appearance or style. New buildings shall be differentiated from the old.*
- *Introducing scale, massing and new materials that are not found within the district.*
- *Cluttering the main entry(s) by "over decorating" with inappropriate features.*



Hatched Buildings Illustrate Appropriate Historic District Setbacks and Building Alignment

### Compatible New Construction

The development of Lake Worth occurred principally in two phases, from the 1910s to the mid-1920s and then again from the mid-1930s to the 1950s. Lake Worth includes a diverse set of architectural styles reflecting the evolving architectural thoughts of these times. Architecture as an art continues to evolve. Appropriate and creative solutions are encouraged in the design of new construction within the Historic District.

Construction of new buildings within the Old Town Historic District will have a positive revitalizing impact on the district if steps are taken to assure that the historic architectural and cultural features of the district are preserved and enhanced. The focus of design guidelines is on the compatibility of new construction within the existing character of the district without dictating style.

The term "compatible design" refers to architectural design and construction that will fit harmoniously into the Historic District. Most new construction in the Old Town Historic District will be in the form of infill on a site adjacent to existing buildings.

### Guidelines For New Construction

New construction should complement the historic architecture. Through sound planning and design, it can reinforce and respect the existing patterns of a historic district. Successful infill design does not have to imitate to be successful. Rather, it picks up significant character-defining features, such as height, materials, roof form, massing, set-back and the rhythm of openings to insure that a new building blends with its context.

While the Secretary of the Interior's Standards are oriented toward rehabilitation of existing historic buildings, new construction in historic districts is discussed in the Standards. The relationship of the new construction to adjacent buildings, landscape and streetscape features, and open spaces should be appropriate and continuous. New construction adjacent to historic buildings can dramatically alter the historic setting of neighboring buildings or the district. Such construction should not create a false sense of historical development through the use of conjectural features or stylistic elements drawn from other buildings. New construction is appropriate as long as it does not destroy significant historic features, including designed landscapes, and complements the size, color, material and character of adjacent buildings and their historic setting.

**The following criteria will be used when reviewing new construction in historic districts:**

**Height:** The height of buildings in most districts, particularly at the street level, is similar.

- *The height of new construction should be compatible with surrounding historic buildings.*

**Width:** Building or lot width is another important visual quality. This often results in common-sized buildings and a characteristic rhythm.

- *The width of new construction should be compatible with surrounding historic buildings.*

**Setback:** Buildings in the Old Town Historic District often share a common front and side setback. Commercial buildings are generally set directly on property lines, creating a wall effect.

- *In locating new buildings the front side and rear setbacks should be maintained and aligned with the facades of surrounding historic buildings.*

**Scale:** The proportion of a new building and the major relationship to neighboring buildings are components in establishing compatibility within the district.

- *The height width ratio; that is, the relationship between the height and width of the front façade (in the case of corner lots, two facades), should be of similar proportion to the neighboring buildings.*



Historic Postcard

Scale and Massing in a Large Building  
Lauiston Building c. 1925 (since demolished)  
Southeast corner of Dixie and Lake Avenue

**Roof forms:**

- *Roof designs including parapet roofs should be compatible with surrounding buildings.*
- *Roof materials should be compatible or the same as materials seen on roofs in the historic district.*

**Materials:** Certain materials are characteristic of historic districts.

- *Materials that are compatible in quality color texture finish and dimension to those common to the district should be used.*

# Design Guidelines for Major Thoroughfares



K Street within Historic  
District



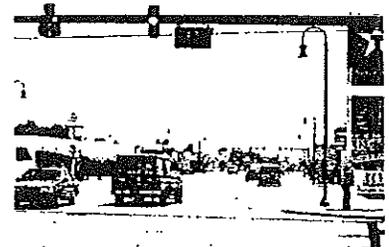
View of 10<sup>th</sup> Avenue North



View of  
Federal Highway



Historic Postcard  
Lake Avenue



Dixie Highway at  
10<sup>th</sup> Avenue North  
Looking South

Abell Garcia Architects

**Guidelines For New Construction**

**Recommendations:**

- *Design new buildings to be compatible in materials, massing and window configurations to the surrounding buildings. See the Old Town Historic District Materials Palette.*
- *Add a new building that is compatible to the height and width of buildings on adjacent sites.*
- *Integrate a new building wider than the buildings on adjacent sites by breaking the building mass or dividing the building width to conform to building widths on adjacent sites.*
- *Match the setbacks of the historic buildings on the street.*
- *If parking is provided it should be at the rear of the building.*

**Avoid:**

- *Designing new buildings whose massing and scale are inappropriate and whose materials and texture are nonhistoric.*
- *Imitating an earlier style or period of architecture in new construction. New buildings shall be differentiated from the old.*
- *Adding a new building to a site that does not maintain or suggest the widths of building on adjacent sites.*

**Demolition**

Demolition exerts a negative impact not only in a historic district, but in the commercial core of downtown. Eliminating a building from a streetscape is like pulling teeth. Either a conspicuous void is created, or the replacement is usually less well-designed and constructed than the original.

In some instances demolition may be appropriate. Nonhistoric buildings whose designs are not in character with their surroundings may be removed. Demolition of nonsignificant additions may be appropriate. Demolition may be undertaken if the addition is less than fifty years old; does not exhibit fine workmanship or materials, was added after the period of significance of the building or district, is so deteriorated it would require reconstruction, or obscures earlier significant features.

**The Historic Preservation Ordinance for the City of Lake Worth (Section 23.27.05.10) describes the requirements and procedures for a demolition permit.**

**Recommendations:**

- *Identify, retain, and preserve buildings that are important in defining the overall historic character of a historic district or district.*
- *Remove nonsignificant buildings, additions, or site features that detract from the historic character of a site or the surrounding district or districts.*
- *Permitted plans for replacement required before demolition.*

**Avoid:**

- *Removing buildings that are important in defining the overall historic character of a district or neighborhood so that the character is diminished.*

## DESIGN GUIDELINES FOR MAJOR THOROUGHFARES

### Major Thoroughfares

The design guidelines for new and renovated development along major thoroughfares include the following areas:

- Lake and Lucerne Avenues from the Intracoastal to I-95 and within the Old Town Historic District
- H, J, K, L and M Streets within the Old Town Historic District
- 10th Avenue North from Dixie Highway to I-95
- 6th Avenue South from Dixie Highway to I-95
- Federal Highway from the South City Limit to the South Boundary of College Park
- Dixie Highway from the South City Limit to 10th Avenue North

The streets listed above are the major commercial thoroughfares in the City of Lake Worth. Guidelines for the major thoroughfares are complementary to the guidelines for the Old Town Historic District.

The Old Town Local Register Historic District is three blocks deep and seven blocks long, though the boundary is irregular. The length of the district is bounded by G Street North to the west and Federal Highway to the east. From north to south the district stretches from a half block north of Lucerne Avenue to 1st Avenue South. The Old Town National Register Historic District is contained completely within the boundary of the Local district. See Maps 1 and 2 at the beginning of the document for the boundaries of the Old Town Historic Districts.

**Banyan Tree, Q Street, Lake Worth, Florida**



Historic Postcard  
Lake Worth

## **Design Guidelines for Property Owners on the Major Thoroughfares**

The following guidelines apply to all properties located along the sections of major thoroughfares listed here:

- Lake and Lucerne Avenues from the Intracoastal to I-95 and within the Old Town Historic District
- H, J, K, L and M Streets within the Old Town Historic District
- 10th Avenue North from Dixie Highway to I-95
- 6th Avenue South from Dixie Highway to I-95
- Federal Highway from the South City Limit to the South Boundary of College Park
- Dixie Highway from the South City Limit to 10th Avenue North

**Within the boundaries of the Old Town Historic District, the Historic District Guidelines take precedence over the Major Thoroughfare Guidelines for properties located within the Historic District.**

**All buildings and building materials must meet all applicable local, state and federal codes and/or requirements.**

### **Index to Major Thoroughfare Design Guidelines**

#### *Site Conditions*

- Setbacks and Orientation page 35
- Buffers page 37
- Parking Design page 38
- Parking Garages page 38
- Hedge, Wall and Fence Design page 39
- Walkways page 39
- Utilities page 40
- Lighting page 40
- Plant Palette page 41

#### *Building Conditions*

- Building Façade and Massing page 43
- Building Materials page 44
- Building Heights page 45
- Roofing Design page 46
- Roofing Material page 47
- Color page 47
- Awnings page 47
- Signage page 48

**Setbacks and Orientation**

Setbacks should generally be consistent with adjacent buildings. There are some buildings along these Major Thoroughfares that do not conform to the usual setbacks and do not provide a good model for infill development, such as service stations and convenience stores, in which all of the parking is located in front of the building. However, even those uses can exist compatibly if they reflect the usual building placement by use of a hedge, wall or fence.

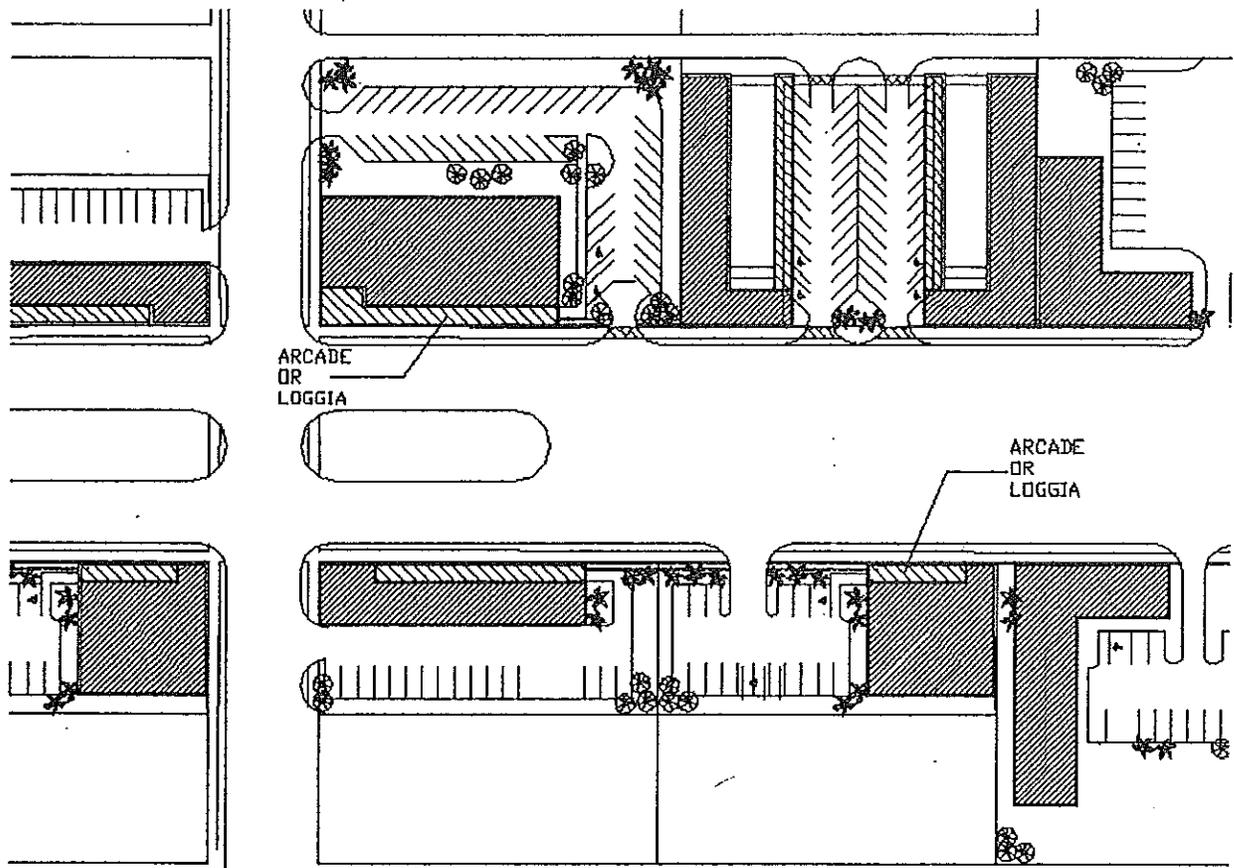


Typical Setbacks along Dixie Highway near Downtown

In areas where many of the older buildings are intact, the setbacks are more consistent, with buildings being set closer to the street edge, usually at the property line. Setbacks should be consistent and new construction shall be set as close to the right of way as possible, in order to create a consistent building frontage suitable for an urban area and to infill the historic fabric of Lake Worth.

Lake and Lucerne Avenues	H, J, K, L, M Streets	10 <sup>th</sup> Ave North and 6 <sup>th</sup> Ave South	Federal Highway	Dixie Highway
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<b>Front Setbacks</b>				
<i>Outside of the Old Town Historic Districts, buildings shall have a 0-foot front setback, unless an arcade or loggia is provided</i>		<i>Buildings shall have a maximum 15-foot front setback unless an arcade or loggia is provided</i>	<i>Buildings shall have a maximum 20-foot front setback unless an arcade or loggia is provided</i>	<i>Buildings shall have a maximum 10-foot front setback unless an arcade or loggia is provided</i>
<i>Inside the Old Town Historic Districts, the front setbacks must be the same as those of the adjoining or closest historic contributing buildings</i>		<i>If the adjacent buildings are closer to the front property line than the above described setback, then the new building shall have the same setback as the adjoining buildings</i>		
<i>If there is an arcade or loggia, the arcade or loggia shall be built to the property line</i>				
<b>General Requirements</b>				
<i>Buildings, arcades or loggias may not obstruct the visibility triangle required at intersections</i>				
<b>Entry Location</b>				
<i>The primary entrance must be located on the Lake or Lucerne Avenue side of the building</i>	<i>The primary entrance must be located on H, J, K, L, or M Street unless the primary façade can be considered to be on Lake or Lucerne Avenues</i>	<i>The primary entrance must be located on 10<sup>th</sup> Ave North or 6<sup>th</sup> Ave South</i>	<i>The primary entrance must be located on Federal Highway unless the primary façade can be considered to be on Lake or Lucerne Avenues</i>	<i>The primary entrance must be located on Dixie Highway unless the primary façade can be considered to be on Lake or Lucerne Avenues</i>



Site Plan Possibilities

**Side Setbacks**

*If the building is located on a corner lot, then the facade facing the side street shall have the same setback as the primary facade*

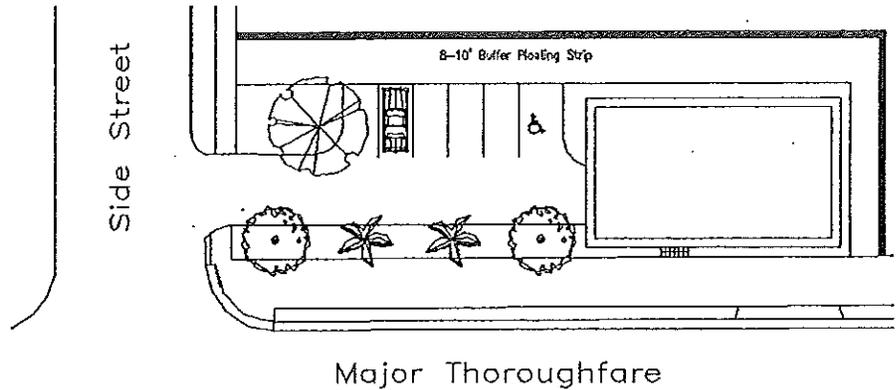
*Side setbacks shall be zero to the interior side lot line, unless parking access is only available from the Major Thoroughfare, in which case the setback is the width required for parking access and the required planting buffer*

*Side setbacks shall be a **maximum** of 10' to the interior side lot line, unless parking access is only available from the Major Thoroughfare, in which case the setback is the width required for parking access and the required planting buffer*

*If the lot is adjacent to a residential lot, then the side setbacks must meet the buffer requirements below*

**Buffers**

When a nonresidential property abuts a residential property, a buffer must be created.



<b>Buffer Requirements</b>
<i>A wall six feet in height along all sides of the property that directly abut a residential use</i>
<i>Six-foot minimum of buffer plantings</i>
<i>Planting of small trees in the buffer if space permits trees to thrive</i>

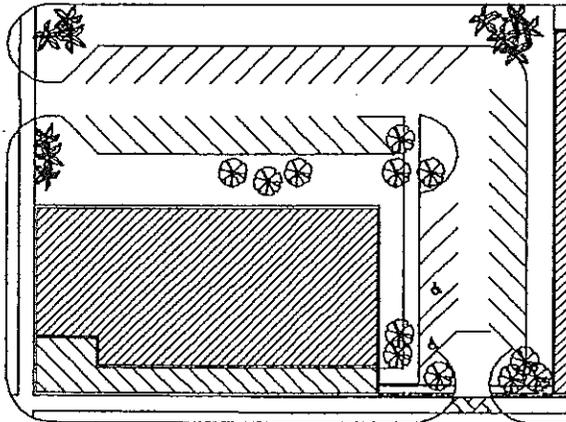
A mix of planting types comprises the buffer. Typically a hedge would line the wall and then trees beyond with ground cover under the trees. The hedge might be up to 24" deep and then another five to eight-feet for the planting of small trees.

<b>Suggested Plant Palette for Buffer Plantings</b>	
<b>HEDGE</b>	
<i>Use: Plantings within buffer strip</i>	
* <i>Forestiera segregata var. pinetorum</i>	Florida privet
* <i>Gyneranthus lucida</i>	Crabwood
<i>Podocarpus macrophyllus maki</i>	Yew podocarpus
<b>TREES</b>	
<i>Use: Planting within buffer strip where buffer width exceeds six-feet</i>	
<i>Psidium littorale</i>	Cattley guava
* <i>Cordia sebestena</i>	Geiger tree
* <i>Cordia boissieri</i>	White cordia
* <i>Guaiacum sanctum</i>	Lignum vitae
<b>PALMS</b>	
* <i>Thrinax radiata</i>	Florida thatch palm
<b>GROUND COVER</b>	
<i>Use: Plantings in buffer strip</i>	
* <i>Nephrolepis exaltata (Boston iensis)</i>	Boston fern
* <i>Tradescantia ohioensis</i>	Spider wort

**Parking Design**

<b>Lake and Lucerne Avenues H, J, K, L and M Streets</b>	<b>10<sup>th</sup> Ave North and 6<sup>th</sup> Ave South Federal Highway Dixie Highway</b>
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<b>Location</b>	
<i>If on-site parking is provided, it is to be located behind the rear facade of the building</i>	<i>All on-site parking shall be located behind the front facade of the building</i>
	<i>Over 50% of the on-site parking provided shall be located behind the rear facade of the building</i>



50% of parking located behind rear Facade of Building

<b>Quantity of Spaces</b>	
<i>In order to encourage the use of arcades or loggias the space covered by the arcade or loggia should not be included when calculating the number of parking spaces required</i>	
<b>Access</b>	
<i>Access shall be from the alley whenever possible</i>	
<i>Access only from the alleys</i>	<i>Access from the side streets and alleys if possible, or from only one point along the Thoroughfare</i>
<i>Access from the Major Thoroughfare is only allowed when there is no other access option</i>	
	<i>Only one ingress/egress point per frontage for lots less than 100' wide</i>
<b>Landscape</b>	
<i>Landscaped with a perimeter planting strip and a thirty-inch hedge along the property line at all points except for ingress/egress points, this landscaping must meet all requirements of the Lake Worth Zoning Code</i>	

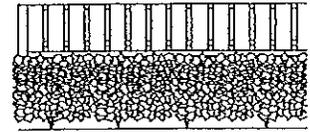
**Parking Garages**

**All Major Thoroughfares**

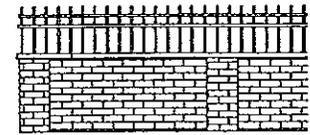
<b>Location</b>	
<i>Located ½ block from major thoroughfares on alleys whenever possible</i>	
<b>Access</b>	
<i>Access shall be from the alley whenever possible</i>	
<i>Access from the Major Thoroughfare is only allowed when there is no other access option</i>	
<b>Mixed Use</b>	
<i>If a parking garage is located on a Major Thoroughfare, the ground floor facing the major thoroughfare and any side streets shall be comprised of retail and commercial spaces</i>	

**Hedge, Wall and Fence Design**

At any place in which the building itself is not immediately adjacent to the right-of-way, a wall, fence or hedge shall be placed immediately adjacent to the right-of-way. The purpose of this is to create a continuous edge along the major thoroughfares in places where there is a break in the built environment.



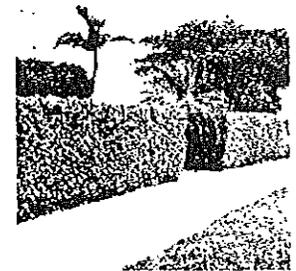
<b>Requirement</b>
<i>At any place in which the building itself is not immediately adjacent to the right-of-way, a wall, fence or hedge shall be placed immediately adjacent to the right-of-way</i>
<i>Hedges are encouraged in lieu of a wall or fence, provided that it is of a type that meets the perimeter planting strip requirements of the Lake Worth Zoning Code and is maintained at the thirty-inch height limit</i>
<i>If a hedge and fence are provided, the hedge shall be placed on the street side of any fence</i>



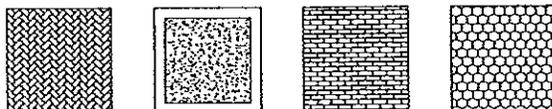
Fence Options

<b>Height</b>
<i>Maximum height of the solid edge shall be thirty inches</i>
<i>Fences up to four-feet tall are allowed as long as they provide visual access above the thirty-inch level</i>
<b>Visibility Triangle</b>
<i>These walls, fences or hedges may not obstruct the visibility triangle required at intersections</i>
<b>Materials</b>
<i>Walls may consist of masonry with a stucco, brick or decorative block finish</i>
<i>Fences may be of painted wood, recycled plastic which resembles wood, iron, cast or decorative aluminum</i>
<i>Chain link fencing is not allowed on the street edge</i>
<i>See the Plant Palette for a listing of suggested hedge plants</i>

Some of the Major Thoroughfares have a dominant planting type already established. For example, along Federal Highway, ficus hedges predominate. Owners are encouraged to plant a similarly structured hedge, though not necessarily a ficus, in order to create a coherent planted edge along this street.



<b>Walkways</b>
<i>A continuous pedestrian walkway at least 6' wide shall be provided from each adjacent perimeter public sidewalk to all customer entrances on commercial buildings</i>
<i>All walkways, including those traversing vehicular use areas shall be constructed of special pavers, bricks, decorative concrete, or other similar pavement treatment</i>
<i>All paving material must meet ADA accessibility guidelines</i>



Suggested Paving Material Types

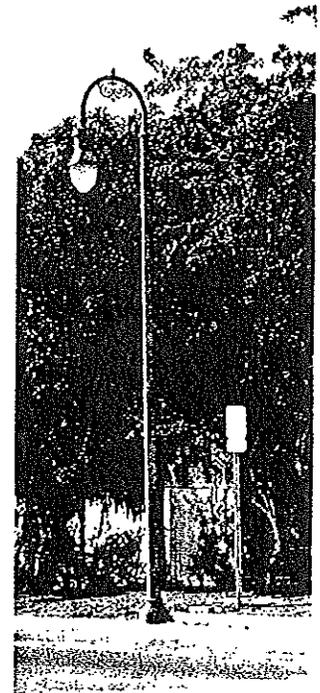
**Utilities and Mechanical Equipment**

<b>Funding</b>
<i>Approval of new development requires that any new projects provide funding for underground utilities on their section of the street</i>
<b>Location</b>
<i>All electrical, mechanical and utility devices, such as back-flow preventors, located on private property must be located behind the front line of the façade of the building and buffered by a wall, fence or hedge from view from all surrounding streets</i>
<b>Screening</b>
<i>All rooftop equipment must be screened from view from all neighboring properties and public streets, this screening shall be of the same material as the building facade</i>

**Lighting**

The redesigned streets within Lake Worth have two types of lighting, an "acorn" and a "teardrop" or "candy cane" fixture. In the commercial areas along the major thoroughfares, it is appropriate to use the same or a similar type of lower-scale lighting fixtures as needed on private land.

<b>Maximum Height</b>
<ul style="list-style-type: none"> <li>• 15' maximum in pedestrian areas</li> <li>• 25' maximum in parking or vehicular use areas</li> <li>• 15' maximum when abutting a residential property</li> </ul>
<b>Installation and Maintenance</b>
<i>Lighting devices for the illumination of the ground, driveways, walkways, parking areas, and building exteriors shall be installed and maintained in such a manner as to confine direct rays to the premises and prevent direct rays and glare onto adjacent properties and rights-of-way</i>
<i>All parking lot and security lighting shall be shielded and directed downward</i>
<b>Design</b>
<i>Designed so as to prevent direct glare, light spillage and hazardous interference with automotive and pedestrian traffic on adjacent streets and all adjacent properties</i>
<i>Lighting and light fixtures are designed to enhance the pedestrian experience</i>
<i>Lighting and light fixtures are integrated and designed so as to enhance the visual impact of the project on the community</i>



Federal Highway Lighting Fixture

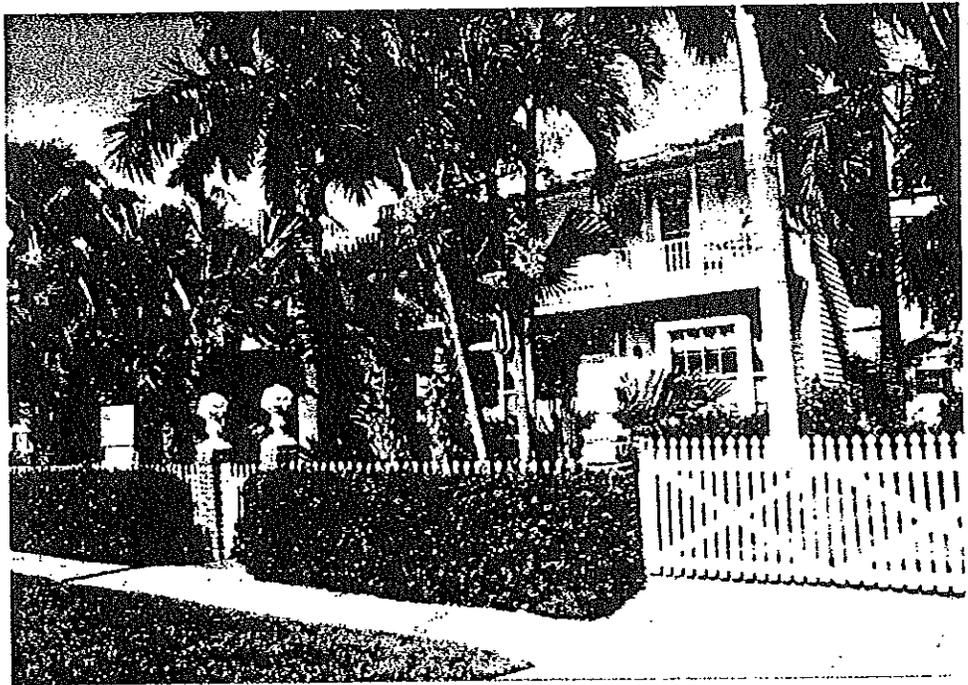
### **Plant Palette**

Lake Worth has a strong component of valuable, historic architecture that establishes the memorable character of the community. The landscape of Lake Worth also contributes to the quality of place. As many promotional materials refer to Lake Worth as the beginning of the tropics, and as the image of exotic plants is strongly associated with southern Florida, many current public and private plantings reflect the general interest in exotic materials. In some instances, these plant materials have been used in southern Florida for so long, that people are often unaware that these materials actually originate in India, China, West Africa, and other parts of the world.

The architectural guidelines emphasize local traditions and techniques and establish methods for filling in the blanks with architecture that is compatible and contextual. The landscape guidelines also look to local material, suggesting primarily native materials to a similar purpose.

Given the large amount of exotic materials in place, the landscape guidelines establish a foundation that allows the exotics to stand out.

Since most property owners are aware of the popular exotics, these plants are not included in the guidelines. Instead, with the potential for



drought, storm, and disease that can afflict exotics more severely than native plants, the guidelines focus on native materials and well-established exotics. The palette does not preclude the use of other materials, nor is the palette intended to replace any current plantings.

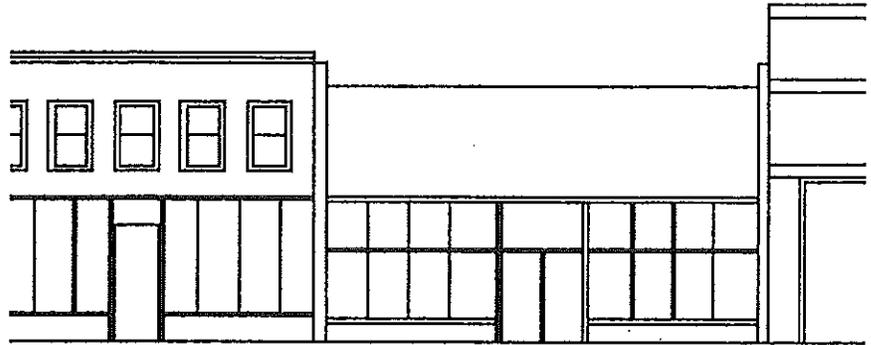
The landscape guidelines offer property owners lists of materials that are generally native or have shown an ability to survive southern Florida with relatively low maintenance. The palette includes some flowering trees and shrubs with the recognition that flowering materials involve more maintenance. In general both the architectural and landscape guidelines endorse indigenous materials as a way of supplementing and strengthening the character that is unique to Lake Worth.

**Plant Palette for Property Owners**

		Lake Avenue and Lucerne Avenues	H, J, K, L, M Streets	10 <sup>th</sup> Ave North and 6 <sup>th</sup> Ave South	Federal Highway	Dixie Highway
<b>HEDGES</b>						
<i>Use: 30" tall, property line</i>						
* <i>Chrysobalanus icaco</i>	Hobe Sound dwarf cocoplum	X	X	X	X	X
* <i>Forestiera segregata var. pinetorum</i>	Florida privet	X	X	X	X	X
<i>Gardenia jasminoides</i>	Gardenia	X	X	X	X	X
<i>Murraya paniculata</i>	Orange jasmine	X	X	X	X	X
<i>Ilex vomitoria 'Nana'</i>	Dwarf Yaupon holly	X	X		X	X
<i>Ixora 'Nora Granti'</i>	Ixora Nora Grant		X		X	X
<i>Pittosporum tobira compacta</i>	Japanese pittosporum		X		X	X
<i>Podocarpus macrophyllus maki</i>	Yew podocarpus maki		X	X	X	X
<b>TREES</b>						
<i>Use: As shade and buffer trees on private property</i>						
<i>Bulnesia arborea</i>	Bulnesia		X	X	X	X
<i>Cassia javanica</i>	Pink and white shower		X	X		
* <i>Chrysophyllum oliviforme</i>	Satin leaf	X	X	X		
* <i>Cordia boissieri</i>	White cordia	X	X	X	X	X
* <i>Cordia sebestena</i>	Geiger tree	X	X	X	X	X
* <i>Krugiodendrum ferreum</i>	Black iron wood	X	X	X	X	X
* <i>Guaiaacum sanctum</i>	Lignum vitae		X	X		
<i>Koelreuteria elegans</i>	Goldenshower		X	X		
* <i>Quercus virginiana</i>	Live oak	X	X	X	X	X
* <i>Swietenia mahagoni</i>	Mahogany	X	X	X	X	X
<i>Tabebuia heterophylla</i>	Pink tab	X				X
<b>PALMS</b>						
<i>Use: Accents among Street Trees or groupings in yard setbacks</i>						
* <i>Coccothrinax argentata</i>	Silver palm	X	X	X	X	X
<i>Coccothrinax miraguama</i>	Miraguama palm		X	X	X	X
* <i>Pseudophoenix sargentii</i>	Buccaneer palm		X	X	X	X
* <i>Roystonea elata</i>	Florida royal palm			X	X	X
* <i>Sabal palmetto</i>	Sabal palm	X	X	X	X	X
* <i>Thrinax radiata</i>	Florida thatch palm	X	X	X	X	X
(* indicates native materials)						

**Building Facades and Massing**

Buildings should reflect the character, scale and fenestration characteristics of Lake Worth. In the Historic District and in areas near the downtown portion of Lake Worth, one building example is the typical commercial storefront, which has an upper facade and a lower facade. The upper facade typically contains large regularly spaced windows and a corbelled or simple cornice. The typical lower facade would contain the storefront, usually topped with a simple cornice, transom windows, a kick plate, and a recessed entrance. The Rowe Building, located at 811 Lake Avenue, is an example of the typical two-part storefront.



Typical Two-Part Storefronts

The Rowe Building, located at 811 Lake Avenue, is an example of the typical two-part storefront. The upper facade of the Rowe Building contains large, arched windows and a barrel tile pent roof below the cornice, as well as the above features on the lower portion of the building.

**Infill Development:**

- buildings shall be similar in size, massing and height to adjoining buildings
- buildings that are larger in size than the adjoining buildings must be articulated and divided into massing that is in proportion to the mass and scale of adjoining buildings

**Primary Façade:**

- the primary entrance side of the building must face the Major Thoroughfare

**At least two of the following features on the Primary Façade:**

- arcades, overhangs or entry features which provide shade
- canopies or porticoes
- balconies
- arches
- display windows or storefronts
- doors with glass insets
- regularly spaced windows
- material changes
- ornamental or structural architectural details such as pilasters or columns



Lake Worth Historic Façade

The addition of porches, arcades, loggias, and canopies further distinguish the architecture and provide shade and protection for the pedestrian. The enhancement of the pedestrian experience encourages commercial participation.

Outside the Historic District, there are a large variety of typical building types. There are many small motels and apartment buildings along both Dixie and Federal Highways, mainly one and two stories in height. There are many small commercial buildings lining Dixie Highway. These typically are diverse in architectural style, but do have some unifying features. Most are of masonry construction, usually with flat roofs and multiple window openings along the highway side of the buildings. These buildings often have strong horizontal elements and large expanses of glass, typical of 1940s and 1950s building styles. The strong horizontality of the building facades ties together the many diverse architectural styles along the major thoroughfares.

<b><i>In addition to the requirements on the preceding page, buildings with over 50' frontage on a Major Thoroughfare:</i></b>
• <i>buildings shall have elements of varying heights and widths</i>
• <i>the building appears to be divided into distinct massing elements and details, along at least 50 percent of the primary facade</i>
• <i>articulated facades should consider a 25-foot increment as the maximum distance for a storefront</i>
• <i>facades that extend beyond 25 feet should be organized into 25-foot increments in order to enhance the pedestrian experience</i>
• <i>buildings on corner lots facing two streets shall provide two articulated facades</i>

***Building Materials***

Lake Worth has a typical palette of building materials, which should be maintained. High-quality exterior building materials are required.

The finish materials of the buildings on the thoroughfares should convey the quality and character of the business community. More refined material should be favored over the rustic. Stucco over concrete, for example, should prevail over exposed concrete block. Overlapping horizontal siding should prevail over board and batten. In all cases, the more refined finish is appropriate for these areas.

<b><i>Acceptable Building Materials</i></b>
• <i>stucco, brick, textured and/or tinted concrete masonry units, treated wood, and cut stone</i>
<b><i>Unacceptable Building Materials</i></b>
• <i>smooth-faced concrete block, tilt-up concrete panels, and prefabricated metal panels and exterior insulation and finish systems</i>

**Building Heights**

Building heights should be consistent or complementary to the existing neighborhood fabric. The existing building fabric in Lake Worth exhibits a large variety of building heights. As greater disparity in building heights is introduced, more of an effort to break up the scale and massing to be compatible with the existing fabric of Lake Worth will be required, yet this urban density is important in infill development.



Lake Worth Hotel - 321 So. Dixie - Lake Worth, Fla. - 1922-3245 - A1 & Belle Lomax, Newport

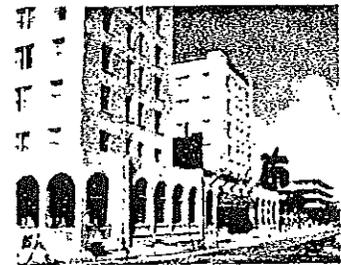
<b>Lake Avenue and Lucerne Avenues</b>	<b>H, J, K, L, M Streets</b>	<b>10<sup>th</sup> Ave North and 6<sup>th</sup> Ave South</b>	<b>Federal Highway</b>	<b>Dixie Highway</b>
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<b>Within the Old Town Historic Districts</b>			
Height shall be two-story (25') minimum and three-story (35') maximum measured to the top of roof			Height shall be two-story (25') minimum and three-story (35') maximum measured to the top of roof

<b>Outside the Old Town Historic Districts</b>			
West of Dixie Highway Maximum height shall be six-story (65') measured to the top of the roof	Maximum height shall be three-stories (35') measured to the top of roof	Maximum height shall be four-stories (45') measured to the top of the roof	West Side of Dixie Highway Maximum height shall be six-story (65') measured to the top of the roof
East of the Districts Maximum height shall be four-story (45') maximum measured to the top of the roof			East Side of Dixie Highway Maximum height shall be four-story (45') maximum measured to the top of the roof

**General Requirements**  
Towers or decorative elements (with a footprint of no more than 200 square feet) shall not be occupiable space, but may extend above the height limits

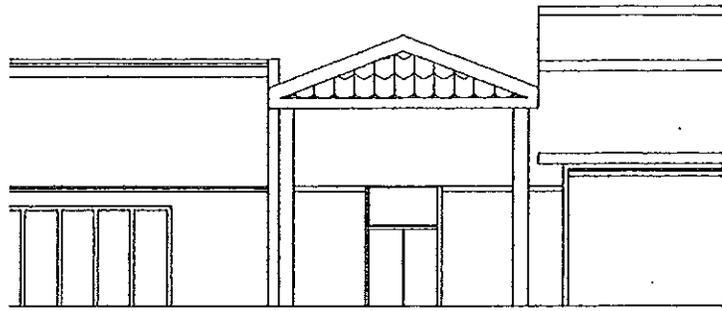
Buildings adjacent to another shorter building shall create changes in the facade and massing to indicate breaks which reflect those of the adjoining buildings



Buildings taller than the adjoining buildings shall be articulated and subdivided into massing that is proportional to the mass and scale of adjoining buildings

**Roof Design**

Roof type, roof edge and parapet treatments are important aspects of the buildings' presence. Mediterranean-influenced buildings are common in Lake Worth and display roof types that are a combination of hip and flat roofs, or a parapet, often with curvilinear features. Classical Revival buildings often have low-pitched hip roofs or a flat roof with a balustrade. The typical roof seen on Commercial and Moderne buildings in Lake Worth is a flat roof with a parapet. New buildings should have roofs that are consistent with the roof types found in the historic buildings of Lake Worth.



Parapets Concealing Flat Roof

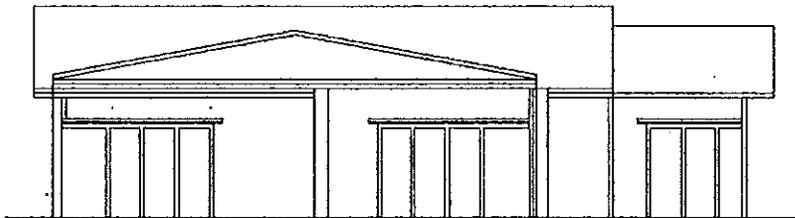
Flat-roof urban buildings have a tradition of embellished parapets that distinguish the business establishment. In an earlier era, the decorative motif of the parapet might have related to the nature of the business. Today, that correspondence is less essential, but the use of the embellished parapet is important to the refined quality of the commercial thoroughfares.

**Flat Roofs**

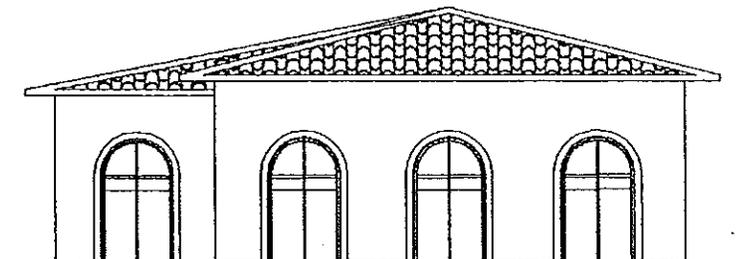
- Flat-roof buildings shall have an embellished parapet which conceals the flat roof and hides all mechanical and electrical equipment from view from the Major Thoroughfares
- If appropriate to the architectural style of the building, three-dimensional cornice treatments shall be used

**Sloping Roofs**

- Overhangs shall be at least three-feet beyond the supporting walls with at least an eight-inch fascia
- On buildings over 50-feet in length, sloping roofs shall have at least three separate roof planes on the facade facing the Major Thoroughfare
- Awnings used as mansard roofs are not allowed



Illustrations of Multiple Roof Planes



**Roofing Materials**

Quality roofing materials are important to the character of the City. Clay tile roofs are one of the identifying characteristics of Lake Worth. When the roof is visible it is important to use materials that respond to the traditions of Lake Worth. For instance, clay tile roofs are appropriate on Mediterranean-influence buildings, while metal standing seam or shingle roofs are appropriate to the Frame Vernacular building style.

<b>Acceptable Materials for Visible Roofs</b>
• <i>clay tile</i>
• <i>standing seam metal panels</i>
• <i>patterned interlocking metal shingles</i>
• <i>slate</i>
• <i>synthetic slate shingles made from recycled materials</i>
• <i>flat cement roof tiles</i>

**Color**

All buildings located on the major thoroughfares are subject to the City's paint approval permitting process. This City of Lake Worth Code limits trim colors to two and base color to one.

<b>Acceptable Colors:</b>
• <i>low reflective, subtle neutral pastels, white, or earth colors</i>
<b>Unacceptable Colors:</b>
• <i>color used as a corporate logo, high-intensity, black, or metallic colors</i>

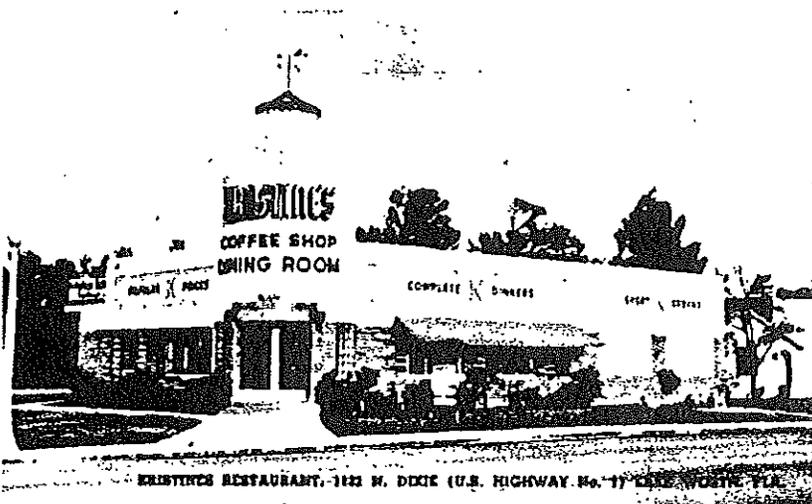
**Awnings**

Awnings were sometimes important functional features on buildings in Florida. They are decorative and appropriate to many buildings, particularly Mediterranean-style buildings. Awning types, materials and placement should be carefully coordinated and be compatible with the characteristics of the building.

<b>Placement:</b>
• <i>placed within or above storefront openings</i>
• <i>shall not interfere with existing signs or distinctive architectural elements of the building</i>
• <i>awnings shall follow the lines of window openings</i>
<b>Colors:</b>
• <i>colors shall be coordinated and compatible with the color of the building</i>
<b>General:</b>
• <i>backlit awnings are not allowed</i>
• <i>awnings used as mansard roofs are not allowed</i>
• <i>awnings used as signage are not allowed</i>

**Signage**

Signage shall be integrated and designed so as to enhance the visual impact of the project on the community. Utilizing design elements consistent with those used in the buildings' architecture will lead to an integrated design statement. Signs that are on the facades of buildings should be designed to fit within the architectural features of the building. Quality sign materials should be used. Signs can often be of the same material as the building facade.



Historic Postcard  
Dixie Highway Streetscape  
1132 North Dixie Highway

**General Note**

- All signage along Lake and Lucerne Avenues and along H, J, K, L and M Streets within the Historic District shall be consistent with the signage allowed in the Historic District, in addition to the signage policies outlined for all Major Thoroughfares and the signage permitted by the Lake Worth Code

All signs on Major Thoroughfares are subject to the City of Lake Worth Code in addition to the requirements shown below.

**Sign Materials:**

- brass plates
- carved or professionally painted wood
- applied wood/metal letters
- etched, sandblasted or gilded glass

**Buildings or Sites with Multiple Signs:**

- signs are unified in color, size and material

**Lighting and Mounting Hardware:**

- Is an integral part of the sign

**Lighting:**

- shall be designed so as to prevent direct glare, light spillage and hazardous interference with automotive and pedestrian traffic on adjacent streets and all adjacent properties

## Design Guidelines for City Improvements on the Major Thoroughfares

The City has undergone an incredible transformation in recent years. Much time, money and effort has been spent in upgrading and improving the streetscapes along Lake and Luceme Avenues and Federal and Dixie Highways. At the same time this public investment has taken place, there has been an increased private interest and investment in the City of Lake Worth.

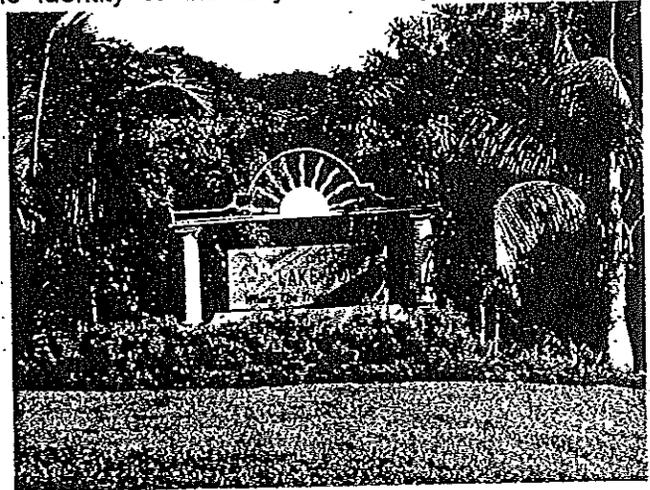
In order to continue and encourage the economic development of the City, there are several improvements the City needs to address over time as capital improvement funds become available. Among these improvements are coordinated signage, entry point enhancements, provision of parking, road narrowing and additional streetscape plantings.

### Signage

One issue to be addressed is a consistent and memorable program of signage and landscaping that will signify to visitors and residents alike that they have now entered the City of Lake Worth. This will help to more clearly establish the identity of the City. This signage should be coordinated along all of the major thoroughfares.

### Entry Points/Gateways to the City

A sign welcoming residents and visitors to Lake Worth should be a prominent part of the landscape design at each of the major entry points to the City. The most significant points of entry to the City are at both the north and south ends of Federal and Dixie Highways, and the points where 6<sup>th</sup> Avenue South and 10<sup>th</sup> Avenue North intersect Interstate-95.



Existing Entry Sign at I-95 and 10<sup>th</sup> Avenue North

The signs need to carry a consistent image and message, and could be modeled on the signs currently located on the south side of 10<sup>th</sup> Avenue North and at 6<sup>th</sup> Avenue South at the I-95 interchanges. Signs at the other entry points should be upgraded to match as budget and right-of-way concerns are addressed.

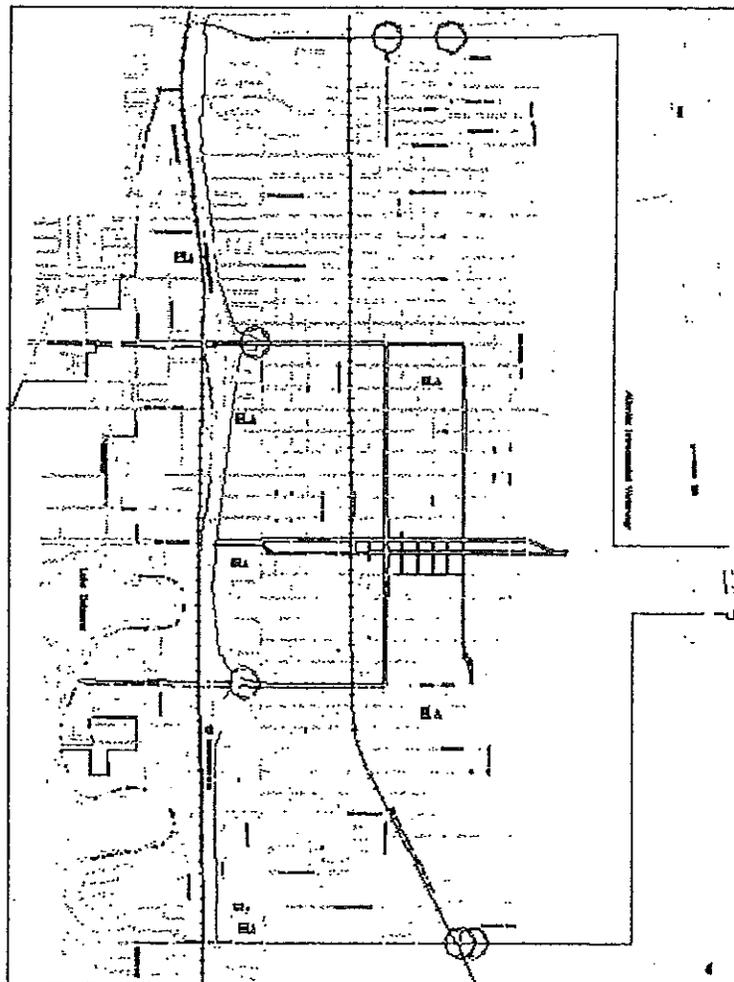
A signature-planting scheme should be established at these strategic entry points, to create or enhance the existing gateway features. The plant palette for these entrances should consist of a focused selection of the following, arranged with the most vertical trees framing the gateway at the street's edge and the smaller species toward the block.

<b>Suggested Plant Palette for City Gateways</b>	
<b>TREES</b>	
* <i>Acacia choriophylla</i>	Cinnecord
* <i>Chrysophyllum oliviforme</i>	Satin leaf
* <i>Coccoloba diversifolia</i>	Pigeon plum
* <i>Cordia sebesteria</i>	Geiger tree
* <i>Swietenia mahagoni</i>	Mahogany
* <i>Quercus virginiana</i>	Live oak

<b>Suggested Plant Palette for City Gateways - Continued</b>	
<b>NATIVE PALMS</b>	
* <i>Coccothrinax argentata</i>	Silver palm
* <i>Roystonea elata</i>	Florida Royal palm
* <i>Sabal palmetto</i>	Sabal palm
* <i>Thrinax radiata</i>	Florida thatch palm
<b>GROUND COVERS</b>	
* <i>Commelina erecta</i>	Dayflower
* <i>Dichromena spp.</i>	White-bracted sedge
* <i>Gaillardia pulchella</i>	Blanket flower
<i>Juniperus conferta</i>	Dwarf shore juniper
* <i>Lantana ovatifolio var. reclinata</i>	Gold lantana
(* indicates native materials)	

All proposed planting must be in compliance with applicable FDOT regulations, including the visibility triangle requirements. Opportunities to place these signs and signature plantings are as follows:

- Sign and signature planting on the north side of Dixie Highway after it crosses the spillway from West Palm Beach
- Smaller scale sign and planting on both sides of Federal after the Highway crosses the spillway from West Palm Beach
- Sign and signature planting on South Dixie at the southern boundary of Lake Worth
- Smaller scale sign and signature planting on South Federal at the southern boundary of Lake Worth
- An additional sign and signature planting on the north side of 10<sup>th</sup> Avenue North and 6<sup>th</sup> Avenue South at I-95



Map of Significant Entry Points to the City of Lake Worth

**Plant Palette**

Lake Worth has a strong component of valuable, historic architecture that establishes the memorable character of the community. The landscape of Lake Worth also contributes to the quality of place. As many promotional materials refer to Lake Worth as the beginning of the tropics, and as the image of exotic plants is strongly associated with southern Florida, many current public and private plantings reflect the general interest in exotic materials. The landscape guidelines look to local material, suggesting primarily native materials to a similar purpose.

The landscape guidelines offer the City a list of materials that are generally native or have shown an ability to survive southern Florida with relatively low maintenance. The palette includes some flowering trees and shrubs with the recognition that flowering materials involve more maintenance. The landscape guidelines endorse indigenous materials as a way of supplementing and strengthening the character that is unique to Lake Worth.

**Infill Plant Palette for the City of Lake Worth**

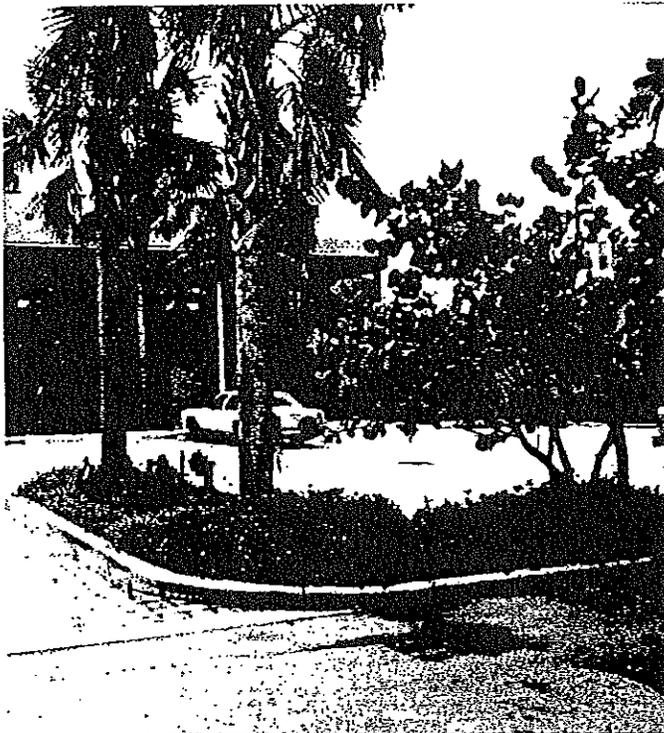
		Lake Avenue and Lucerne Avenue	H, J, K, L, M Streets	10 <sup>th</sup> Ave North and 6 <sup>th</sup> Ave South	Federal Highway	Dixie Highway
<b>HEDGES</b>						
<i>Use: 30" tall, property line</i>						
<i>Chrysobalanus icaco*</i>	Hobe Sound dwarf cocoplum	X	X	X	X	X
<i>*Forestiera segregata var. pinetorum</i>	Florida privet	X	X	X	X	X
<i>Gardenia jasminoides</i>	Gardenia	X	X	X	X	X
<i>Murraya paniculata</i>	Orange jasmine	X	X	X	X	X
<i>Ilex vomitoria 'Nana'</i>	Dwarf Yaupon holly	X	X		X	X
<i>Ixora 'Nora Granti'</i>	Ixora Nora Grant		X		X	X
<i>Pittosporum tobira compacta</i>	Japanese pittosporum		X		X	X
<i>Podocarpus macrophyllus maki</i>	Yew podocarpus maki		X	X	X	X
<b>TREES</b>						
<i>Use: As shade and buffer trees on private property</i>						
<i>Bulnesia arborea</i>	Bulnesia		X	X	X	X
<i>Cassia javanica</i>	Pink and white shower		X	X		
<i>*Chrysophyllum oliviforme</i>	Satin leaf	X	X	X		
<i>*Cordia boissieri</i>	White cordia	X	X	X	X	X
<i>*Cordia sebesteria</i>	Geiger tree	X	X	X	X	X
<i>*Guaiacum sanctum</i>	Lignum vitae		X	X		
<i>Koelreuteria elegans</i>	Goldenshower		X	X		
<i>Quercus virginiana*</i>	Live oak	X	X	X	X	X
<i>*Swietenia mahagoni</i>	Mahogany	X	X	X	X	X
<i>Tabebuia heterophylla</i>	Pink tab	X				X

PALMS						
<i>Use: Accents among Street Trees or groupings in yard setbacks</i>						
* <i>Coccothrinax argentata</i>	Silver palm	X	X	X	X	X
<i>Coccothrinax miraguama</i>	Miraguama palm		X	X	X	X
* <i>Pseudophoenix sargentii</i>	Buccaneer palm		X	X	X	X
* <i>Roystonea elata</i>	Florida royal			X	X	X
* <i>Thrinax radiata</i>	Florida thatch palm	X	X	X	X	X
(* indicates native materials)						

**Parking**

One major concern of business owners is the lack of parking which is seen to limit the potential for growth.

Often in the commercial core of historic downtown there is no space for further on site parking development. As the City is able to fund the projects a list of options would include parking garages and small-scale lots located off of the major thoroughfares. The most feasible current parking solution for downtown would be to create small landscaped parking lots, which are located one-half block from the major thoroughfares.



Alley Access to Parking with Planting Buffer

Because this area is mixed use in nature, it is very important that these lots be landscaped with a larger than usual perimeter planting strip, which is also planted with shade trees to make the transition from these lots to downtown a more inviting pedestrian experience. Large islands of parking would provide barriers to the expansion of downtown commercial expansion and disrupt the mixed-use nature of the surrounding neighborhoods.

As downtown Lake Worth continues to grow, it may become necessary to build multi-story parking garages, located as above, to accommodate further development. The opportunity to build garages, which could include multiple purposes, with retail and commercial spaces located on the ground floors, will help to connect the growth of downtown to the surrounding neighborhoods.

Parking for businesses located along Dixie Highway is another concern of business owners. The same solution of small heavily landscaped lots indicated

above for downtown could be effective in providing the needed parking, while still providing the transition needed to the residential neighborhoods located immediately adjacent to the commercial properties.

**Lake and Lucerne Avenues from the Intracoastal Waterway to I-95 and within the Old Town Historic District**

Lake and Lucerne Avenues are the major thoroughfares in Downtown Lake Worth. Large portions of these roadways are located within the Old Town Historic District. These are the destination and commercial streets comprising the downtown core and the site of major pedestrian activity. As these streets emerge from the Historic District, the nature of the streetscape changes. As one travels toward the Intracoastal Waterway from downtown, building setbacks increase in distance from the street and, currently, the area is more residential in nature. Leaving downtown and moving west, the area currently is mixed-use, including medium and high-density residential, commercial and single-family residences. The Future Land Use Classification for Lake and Lucerne Avenues from the Intracoastal to I-95 is designated "Central Area Commercial," indicating that infill development will be of commercial and mixed use nature.

*Roadways and Utilities*

Lake and Lucerne Avenues have been recently improved and feature 11-foot-wide traffic lanes, on-street parking, and 8, 11 and 21-foot sidewalks within the Historic District. Lake and Lucerne Avenues meet in a traffic circle located at the west end of Lake and Lucerne where the streets terminate at Lake Worth Avenue. Lucerne Avenue also has bicycle lanes. Utilities along Lake and Lucerne Avenue are located underground or in the nearby alleys.

*Streetscape and Landscape*

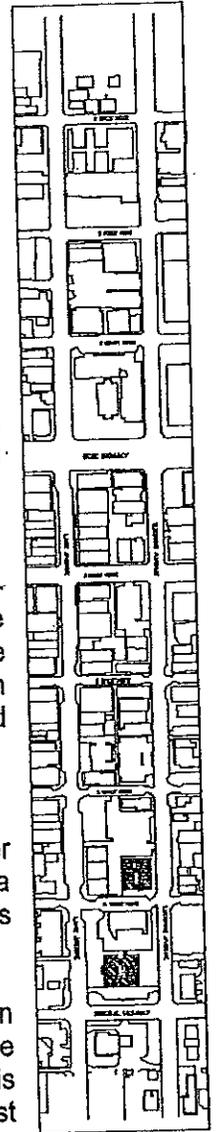
The sidewalks within the Historic District are set with interlocking pavers rather than concrete, an upgrade provided by the City during the recent Florida Department of Transportation improvements to major thoroughfares. This helps to identify the area as a pedestrian zone.



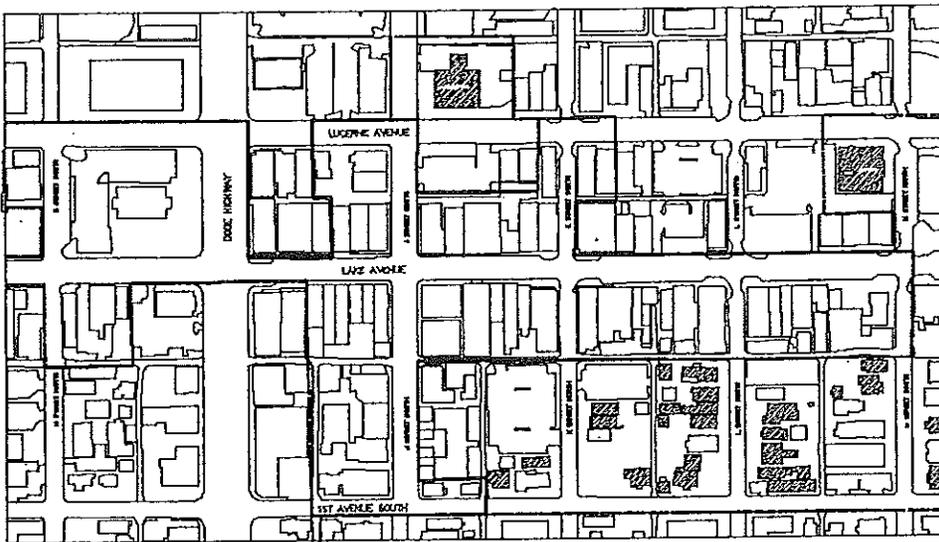
Lake Avenue Streetscape  
Southside of the 500 block

Lake Avenue already has a planting plan developed for the public right-of-way in the downtown core. This planting Palette extends from the Intracoastal to the Florida East Coast Railway. This plan consists of live oaks in the bulb-outs at each intersection, and foxtail palms at twenty-foot intervals along the rest of the street frontage. Lucerne Avenue has live oaks planted at the bulb-outs at the intersections from Federal Highway west to the traffic circle.

Citizen input identified a concern with the lack of consistency in the design of the streetscape on Lake and Lucerne Avenues on the west side of the Florida East Coast Railroad tracks compared with the east side. A specific treatment was intentionally implemented for the area east of the railroad, as this was the traditional historic downtown core. A different palette for enhanced planting could be implemented west of the Railroad, providing an upgrade to the existing streetscape in order to encourage further development along this portion of the roadways.



### H, J, K, L and M Streets within the Old Town Historic District



These five streets are the north-south streets within the Historic District. Each of these streets connects the main arteries of Lake and Lucerne Avenues and provides cross access between these one-way pairs. J Street is a major commercial street at present, with a wide cross-

section and angled parking on two sides, with many businesses fronting the street. The other four streets currently have on-street parallel parking on both sides. The Future Land Use Classification for these streets within the Historic District is Central Area Commercial and Mixed Use. One-way bike lanes connect through two of these north-south streets.

In order to make J Street more pedestrian-friendly and to encourage more commercial activity along the street, some changes need to be made. Pedestrian crossings mid-block, islands in the middle of the street at crossings and more shade along the street would enhance the pedestrian experience and encourage commercial participation.

#### *Roadways and Utilities*

These five roadways are of various widths. There are narrow sidewalks on both sides of the streets and very little shade. Power lines along most of these streets are located underground or in the adjoining alleys.



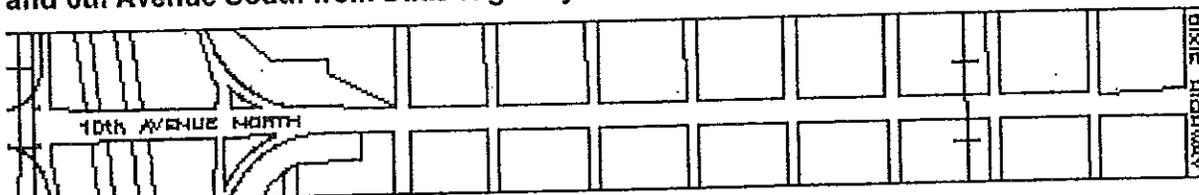
J Street Looking South

Power lines along most of these streets are located underground or in the adjoining alleys. As part of ongoing downtown improvements it should be a priority to place the remaining utilities underground in order to eliminate the lines that currently cross over the streets in this historic core of downtown.

#### *Streetscape and Landscape*

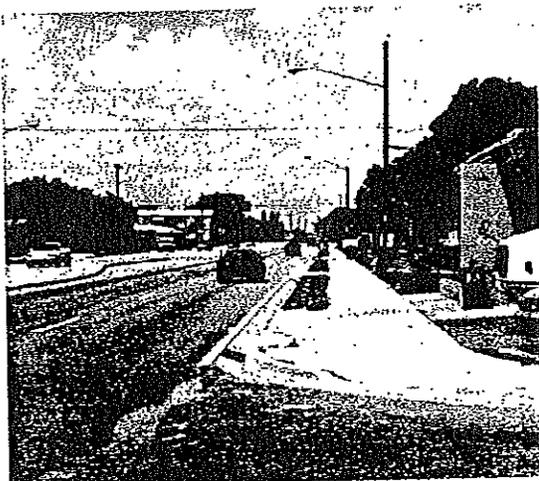
These cross streets require shade and a feeling of connectedness in order to invite pedestrians to walk the streets. Extending the ongoing downtown improvements, shade trees should be planted wherever there is sufficient right-of way or plants in pots placed along the sidewalks. The bulb-outs on these streets that occur at the intersections with Lake and Lucerne Avenues should be planted with shade trees in the public right-of-way. This would help to more clearly connect these streets to the more pedestrian friendly environment found on Lake and Lucerne Avenues.

**10th Avenue North from Dixie Highway to I-95  
and 6th Avenue South from Dixie Highway to I-95**



Both of these avenues serve as primary entryways into the City and need to be designed to create a sense of arrival and a sense of place for the person entering the City of Lake Worth from the Interstate. This is often the first impression a visitor to the City receives. A visual consistency connecting the many uses contemplated will help to ensure a sense of organization and consistency to these avenues.

The Future Land Use Map Classification for 10<sup>th</sup> Avenue North is General Commercial. 10<sup>th</sup> Avenue North includes Mixed-Use Office-Residential from the Interstate east to the railroad tracks and General Commercial from the Railroad tracks to one half block past Dixie Highway. The Future Land Use Map Classification for 6<sup>th</sup> Avenue South is predominantly Mixed-Use Office-Residential from the Interstate to Dixie Highway.



View of 6<sup>th</sup> Avenue South

*Roadways, Streetscape and Landscape*

The overall widths of the streets contribute to the perception of rapid speed and large volume traffic on the major entryways into the City. 10<sup>th</sup> Avenue North and 6<sup>th</sup> Avenue South from the Interstate to Dixie Highway have wide traffic lanes, which allow faster speeds to be attained. The streets would benefit from a narrowing of the lane widths to 11 feet and the expansion of the swale to create a buffer zone of landscape and streetscape features. As an alternative, a median could be used to provide a linear planting scheme, while still accomplishing the goal of narrowing the lanes, slowing traffic, and providing a sense of entry to the City.

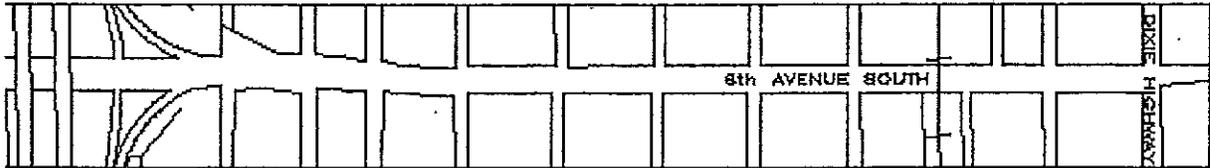
By narrowing the travel lanes, sufficient space for planting of shade trees will be created. Palms in cutouts with pavers have been planted along 10<sup>th</sup> Avenue North. The addition of shade trees would serve to make the pedestrian experience more pleasant. 10<sup>th</sup> Avenue North from Dixie to Interstate-95 has continuous sidewalks, but in many places they are not provided with accessible curb cuts and ramps. 6<sup>th</sup> Avenue South from Dixie to Interstate-95 has seven-foot sidewalks and a three-foot grass strip adjacent to the roadway.

*Lighting*

The use of the same street lighting fixtures along 10<sup>th</sup> Avenue North and 6<sup>th</sup> Avenue South as used along Dixie and Federal Highways would create a unified theme for entry into the City. The current light fixtures are of the type commonly used beside major highways and light the traffic lanes without lending scale to the pedestrian environment. Because these streets are more residential in scale, the lighting levels may need to be reduced from those on Dixie Highway.

*Utilities and Mechanical Equipment*

These two major thoroughfares, 10<sup>th</sup> Avenue North and 6<sup>th</sup> Avenue South, still have overhead utilities that create visual clutter. In the portions of these streets which are used as major gateways to the City, from I-95 to Dixie Highway, the City should make it a priority to provide underground utilities as soon as possible while street improvements are made. Approval of new development should require that any new projects provide funding for underground utilities on their section of the street, whenever feasible.



**Federal Highway from the South City Limit to the South Boundary of College Park**

Federal Highway underwent major improvement with the installation early in 1999 of underground utilities, light fixtures that recall period piece fixtures of the turn-of-the-century and new landscaping. Federal Highway is a two-lane roadway that passes through multiple types of uses. Currently there are single-family residential, multi-family residential, motels, churches, offices and commercial buildings located along this section of Federal Highway. The Future Land Use Classification for Federal Highway from 10<sup>th</sup> Avenue North to 6<sup>th</sup> Avenue South is Mixed-Use Office-Residential.

*Roadways*

The typical street section includes two 12-foot travel lanes, and six-foot sidewalks.

*Lighting*

Federal Highway currently has two types of lighting fixtures, the "acorn" and "teardrop" or "candy cane" installed as part of the recent street improvements.

*Utilities*

Utilities have been placed underground as part of the recent road improvements.

*Streetscape and Landscape*

As part of the street improvements in 1999, this area of Federal Highway received new landscaping. This landscaping includes live oaks and yellow tabebuia and a selection of other trees selected to enhance the palette already seen on private properties along Federal Highway. Further infill of this existing plant palette and a series of coordinated hedges would create a unique sense of place along Federal Highway.



View of Federal Highway

## Dixie Highway from the South City Limit to 10th Avenue North

Dixie Highway underwent major improvement with the installation in 1999 and early 2000 of underground utilities, light fixtures that recall period piece fixtures, consistent sidewalks and new landscaping. The landscape of much of Dixie Highway consists of street trees planted in all areas in which there is sufficient right-of-way. A large median planting program begins as one approaches downtown, with a variety of trees and plants in large groupings.

Dixie Highway serves as the major commercial corridor in the City. Business owners are concerned about the provision of parking and consider that a major issue. The speed limit on this portion of Dixie Highway varies from 25 to 35 mph, yet there is a perception of high speed on this roadway. The Future Land Use Classification for Dixie Highway from 10<sup>th</sup> Avenue North to 6<sup>th</sup> Avenue South is General Commercial.

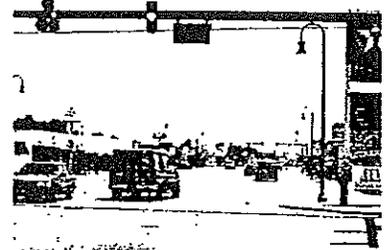
### *Roadways*

The typical street section is four lanes, which includes 11-foot travel lanes, and nine-foot sidewalks. In the downtown core the street section changes, with narrower sidewalks, and large medians.



### *Lighting*

Dixie Highway currently has both the "acorn" and "teardrop" or "candy cane" types of light fixtures installed as part of the recent street improvements.



### *Utilities*

Utilities have been placed underground or in the nearby alleys as part of the recent road improvements.

### *Streetscape and Landscape*

As part of the street improvements in 1999 and 2000, Dixie Highway received new landscaping. This landscaping includes live oaks and yellow tabebuia. The yellow tabebuia are planted wherever there is sufficient right-of-way to allow for planting and the live oaks are placed at intersections where streets have been made one-way. There is also a large grouping of plants in the medians in the approach to downtown, which includes both low-scale planting and larger trees, creating an entry statement for the downtown commercial district.

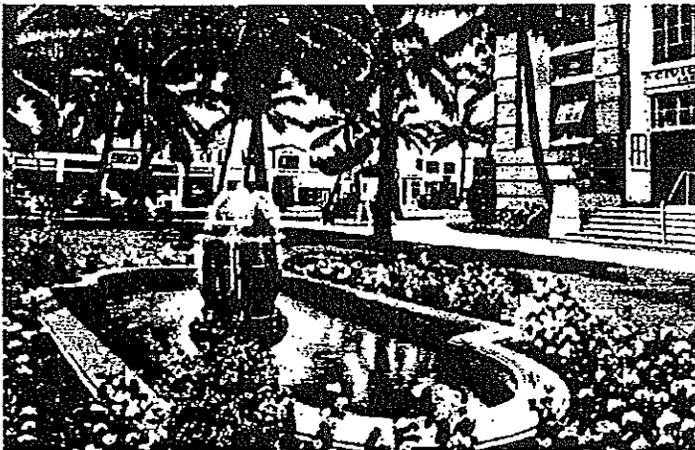


Dixie Highway has a wide road section and much of the existing parking immediately abuts the sidewalks, which creates a hot expanse of concrete and asphalt with no shade. This creates the need for more planting and vegetation along this corridor to provide needed shade for pedestrians. There are many areas where asphalt or concrete parking lots immediately abut the sidewalks. Much of this planting will need to be done by private owners on their properties, as there is already a heavy public planting program on the areas of Dixie where there is sufficient right-of way.

## CONCLUSION

The Old Town Historic District and Major Thoroughfare Guidelines represent an opportunity for improvements to both the public and private sector properties of Lake Worth. The Guidelines are intended to provide developers of new properties and redevelopers with information to guide their design decisions. The Guidelines also provide the City with information on establishing priorities for public improvements. There are a number of significant issues that are important to maintaining the character of Lake Worth while contributing to continued economic growth and development. These priorities include:

- Enhance the identity of the City of Lake Worth
- Create a memorable streetscape and landscape experience
- Maintain the compact downtown core
- Maintain historic scale and character of downtown
- Preserve historic downtown buildings and open spaces
- Maintain variety in building height, facades, materials and rooflines
- Use materials that create an image of consistency and quality
- Create pedestrian friendly, tree shaded connections to public open space and commercial development
- Create a clear division between public and private property
- Maintain diversity of uses, which include residential, retail and office uses in all areas of the City
- Provide parking one block off of major thoroughfares where needed, clearly signed for easy access



Historic Postcard  
Fountain on Dixie Highway (since removed)

Enhancing development in areas such as Lake Worth where infrastructure already exists is important for multiple reasons. It makes the existing infrastructure more cost-effective for new and existing development as it serves more people. It brings more people into the central area of the City that has pedestrian and bicycle amenities, which encourages the use of these amenities and a reduction in the need to drive to services. Development in the City creates a customer base for the retail businesses in the City.

All of the issues above are important in maintaining the diversity, which is characteristic of and valued in Lake

Worth. If opportunities are provided for continuing community input into the redevelopment and urban design process, it can be ensured that the process will be successful in accomplishing the above goals. Continued promotion of downtown activities will draw more people into the public life of Lake Worth. Continued enhancements along the major thoroughfares will provide opportunity for continuing commercial success. Encouraging owners of existing properties to enhance their properties using the suggestions in the Guidelines is one way to provide a more cohesive design environment. The City and its citizens now have the opportunity to prioritize and implement the changes desired in Lake Worth.